APPENDIX A

CONSULTATION AND COMMUNICATION

APPENDIX A.1

Contact List

Agency Contact List

Title	First	Last	Title	Agency	Branch	Address1	Address2	City	Prov	Postal Code	Phone	Email
Elected Officials												
	Karen	Vecchio	MP	MP - Elgin - Middlesex - London		750 Talbot Street	Suite 203	St. Thomas	ON	N5P 1E2	(519) 637-2255	karen.vecchio@parl.gc.ca
Hon.	Jeff	Yurek	MPP	MPP - Elgin - Middlesex - London		750 Talbot Street	Suite 201	St. Thomas	ON	N5P 1E2	(519) 631-0666	jeff.yurek@pc.ola.org
Provincial/Federal Ministries and Agencies												
	Lise	Chabot	Manager	Ministry of Indigenous Affairs and Reconciliation	Ministry Partnerships Unit	160 Bloor Street, East	Suite 400	Toronto	ON	M7A2E6		lise.chabot@ontario.ca
	Karina	Cerniavskaja	District Planner	Ministry of Natural Resources and Forestry	Aylmer District	615 John Street North		Aylmer	ON	N5H 2S8	(519) 773-4757	karina.cerniavskaja@ontario.ca
	Kathleen	Buck	Management Biologist	Ministry of Natural Resources and Forestry	Aylmer District	615 John Street North		Aylmer	ON	N5H 2S8	(519) 773-4785	kathleen.buck@ontario.ca
			Fish Protection Biologist	Fisheries and Oceans Canada	Central and Arctic Region	867 Lakeshore Road		Burlington	ON	L7S 1A1	(855) 852-8320	fisheriesprotection@dfo-mpo.gc.ca
	Erick	Boyd	Manager	Ministry of Municipal Affairs and Housing	Community Planning and Development	659 Exeter Road	2nd Floor	London	ON	N6E 1L3	(519) 873-4025	erick.boyd@ontario.ca
	Mike	Pearsall	Manager (Acting)	Ministry of Transportation	Design and Contract Standards Office	301 St. Paul Street	2nd Floor	St. Catharines	ON	L2R 7R4	905-704-2284	mike.pearsall@ontario.ca
	Phill	Hutton	Manager	Ministry of Transportation	Design and Contract Standards Office	301 St. Paul Street	2nd Floor	St. Catharines	ON	L2R 7R4	(905) 704-2199	phil.hutton@ontario.ca
			Heritage Planner	Ministry of Tourism, Culture and Sport	Culture Services Unit	401 Bay Street		Toronto	ON	M7A 0A7		
Municipal Contacts												
	Kathy	Bunting	County Clerk	Middlesex County		399 Ridout Street North		London	ON	N6A 2P1	(519) 434-7321 Ext 2250	
	Alison	Warwick	Mayor	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3		
	Kelly	Elliott	Deputy Mayor	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3		
	Tena	Michiels	Clerk	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3	(519) 268-7334 ext. 222	tmichiels@thamescentre.on.ca
	Mike	LeBlanc	Director of Transportation	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3	(519) 268-7334 ext. 235	mleblanc@thamescentre.on.ca
	Tom	Heeman	Councillor - Ward 1	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3	(519) 617-7086	theeman@thamescentre.on.ca
	Carlos	Reyes	Director of Environmental Services	Municipality of Thames Centre		4305 Hamilton Road		Dorchester	ON	N0L 1G3	(519) 268-7334 ext. 245	creyes@thamescentre.on.ca
	Cathy	Saunders	City Clerk	City of London	City Clerks Office	300 Dufferin Avenue	PO Box 5035	London	ON	N6A 4L9	(519) 661-2489 ext. 1880	
	Dave	Sumner	Supervisor of Operations	City of London	Transportation and Roadside Operations	663 Bathurst Street		London	ON	N5Z 1P8	(519) 661-2489 ex 5416	dsumner@london.ca
Conservation Authority												
	Karen	Winfield	Land Use Regulations Officer	Upper Thames River Conservation Authority		1424 Clarke Road		London	ON	N5V 5B9	(519) 451-2800 ext. 237	winfieldk@thamesriver.on.ca
	Mark	Snowsell	Land Use Regulations Officer	Upper Thames River Conservation Authority		1424 Clarke Road		London	ON	N5V 5B9	(519) 451-2800	snowsellm@thamesriver.on.ca
	Phil	Simm	GIS	Upper Thames River Conservation Authority		1424 Clarke Road		London	ON	N5V 5B9	(519) 451-2800 ext. 247	
	Mark	Shifflett	Water Resource Enginer	Upper Thames River Conservation Authority		1424 Clarke Road		London	ON	N5V 5B9	(519) 451-2800 ext 239	
	Steve	Musclow	Fanshawe Conservation Area Supervisor	Upper Thames River Conservation Authority		1424 Clarke Road		London	On	N5V 5B9		musclows@thamesriver.on.ca
	Karen	Wilkie	Properties/ Land Management Technician	Upper Thames River Conservation Authority		1424 Clarke Road		London	On	N5V 5B9	(519) 451-2800 ext. 263	wilkiek@thamesriver.on.ca
	Ben	Dafoe	Assistant Superintendent	Upper Thames River Conservation Authority		1424 Clarke Road		London	On	N5V 5B9	226-235-1018	dafoeb@thamesriver.on.ca
Emergency Services												
	Al	Hunt	Deputy Chief, Operations	Middlesex-London EMS Service	Paramedic Service	340 Waterloo Street		London	ON	N6B 2N6	(519) 679-5466 ext. 1105	
Chief	Randy	Kalan	Chief	Thames Centre Fire Department	Fire Chief's Office	2066 Dorchester Rod		Dorchester	ON		(519) 268-7334 ext 712	rkalan@thamescentre.on.ca
				Ontario Provincial Police	Middlesex (London)	823 Exeter Road		London	ON	N6E 1W1	(519) 681-0300	
Local Agencies and Interest Groups												
	Laura	Elliott	Director of Education	Thames Valley District School Board		1250 Dundas Street		London	ON	N5W 5P2	(519) 452-2000	
	Linda	Staudt	Director of Education	London Catholic District School Board		5200 Wellington Road South		London	On	N6E 3X8	(519) 663-2088	
				Southwestern Ontario Study Transportation Services	My Big Yellow School Bus	201-557 Southdale Rd E.		London	ON	N6E 1A2	(519) 649-1160	
	Becky			I Love Thorndale				Thorndale	ON			info@ilovethorndale.ca
	Debbie	Guy	Supervisor	Thorndale Library	Thorndale Library	21790 Fairview Road		Thorndale	ON	N0M 2P0	(519) 461-1150	dguy@middlesex.ca
				Thorndale Community Centre		265 Upper Queen Street		Thorndale	ON	N0M 2P0	(519) 268-7334 ext. 701	
				Thorndale United Church		245 King Street	PO Box 126	Thorndale	ON	N0M 2P0	(519) 609-6132	office@tzuc.ca
Rev.	Patty	Dobbs Luxton	Minister	St. George's Anglican Church		172 King St.		Thorndale	ON		(519) 765-4019	ledwards47@hotmail.com
Rev.	Heather	Paton		South Nissouri Presbyterian Church		17358 Oliver Road		Thorndale	ON	NOM 2P0	(519) 268-3399	heapat@outlook.com
				Canadian Heritage River System	Local Interest Group							heritagerivers@pc.gc.ca
				London Cycling Club	Local Interest Group							londoncyclingclubinfo@gmail.com
	Lorne	Falkenstein	President	London Centennial Wheelers	Local Interest Group							ONLINE EMIAIL FORM ONLY
	Alex	Vanderkam		Thames Valley Trail Association	Local Interest Group	1017 Western Road	c/o Grosvenor Lodge	London	ON	N6G 1G5	226-980-6404	president@tvta.ca
	Todd	Sleeper		Friends of the Thames								td.sleeper@hotmail.com
				Thames River Anglers Association	Local Interest Group	2202 Coronation Drive		London	ON	N6G 0B9		traa@anglers.org
	Tilman	Joosten		Thames Valley Trail Association	Local Interest Group	210 Martin Ave.		London	ON	N5V 4K2	519-245-4862	
Utilities												
	Jim	Walker	Trade Supervisor & Locate Contact	Hydro One		850 Pond Mils Road		London	ON	N5Z 4R2	(519) 649-3664	jim.walker@hydroone.com
	Jamie	MacPherson	Engineering Technologist	London Hydro			PO Box	London	ON	N6A 4H6	(519) 661-5800 ext. 5364	macpherj@londonhydro.com
	Jerry	LaCount	System Planner	Rogers Communications		800 York Street		London	ON	N5W 2S9	(519) 619-2366	jerry.lacount@rci.rogers.com
	Patrick	Hunt	Project Coordinator	Bell Canada	Access Network Facilities	725 Colborne Street	3rd Floor	London	ON	N6A 2Z6	(519) 850-6533	patrick.hunt@bell.ca
	Rob	Elliot	Construction Project Manager	Union Gas Limited/Spectra Energy Company		109 Commissioners Road West		London	ON	N6A 4P1	(519) 667-4100 ext 515-3512	roelliot@uniongas.com

Project No. 165001122

Thorndale Bridge Improvements
Municipal Class Environmental Assessment, County of Middlesex

Indigenous Communities Contact List

Title	First	Last	Title	Agency	Branch	Address1	Address2	City	Prov	Postal_Code	Email
Indigenous Communities											
Chief	Jacqueline	French	Chief	Chippewas of the Thames		320 Chippewa Road RR1		Muncey	ON	N0L 1Y0	jfrench@cottfn com
Ms.	Rochelle	Smith	Consultation Coordinator	Chippewas of the Thames	Lands and Resources	320 Chippewa Road RR1		Muncey	ON	N0L 1Y0	rsmith@cottfn.com
Ms.	Fallon	Burch	Consultation Manager	Chippewas of the Thames		320 Chippewa Road RR1		Muncey	ON	N0L 1Y0	consultation@cottfn com
Chief	Jessica	Hill	Chief	Oneida Nation of the Thames		2212 Elm Avenue		Southwold	ON	N0L 2G0	jessica.hill@oneida.on.ca
Ms.	Cherilyn	Hill	Political Office Manager	Oneida Nation of the Thames		2212 Elm Avenue		Southwold	ON	N0L 2G0	cherilyn.hill@oneida.on.ca
Mr.	Brandon	Doxtatro	Environment Coordinator	Oneida Nation of the Thames		2212 Elm Avenue		Southwold	ON	N0L 2G0	environment@oneida on.ca
Chief	Roger	Thomas	Chief	Munsee-Delaware Nation		289 Jubilee Road		Muncey	ON	N0L 1Y0	chief@munsee.ca
Mr.	Glenn	Forrest	Band Manager	Munsee-Delaware Nation		289 Jubilee Road		Muncey	ON	N0L 1Y0	glenn@munsee.ca
Ms.	Stacey	Phillips	Consultation	Munsee-Delaware Nation		289 Jubilee Road R.R #1		Muncey	ON	N0L 1Y0	consultation@munsee.ca
Chief	Denise	Stonefish	Chief	Delaware Nation (Moravian of the Thames)		14760 School House Line RR #3		Thamesville	ON	N0P 2K0	denise.stonefish@delawarenation.on.ca
Ms.	Janet	Macbeth	Project Review Coordinator	Bkejwanong Territory (Walpole Island)		117 Tahgahoning Road RR #3		Walpole Island	ON	N8A 4K9	janet.macbeth@wifn.org
Ms.	Nikki	Orosz	Director of Operations	Caldwell First Nation		14 Orange Street	P.O.Box 388	Leamington	ON	N8H 1P5	nikki.orosz@caldwellfirstnation.ca
Chief	Chris	Plain		Aamjiwnaang First Nation		978 Tashmoo Avenue,		Sarnia	ON	N7T 7H5	chief.plain@aamjiwnaang.ca
Chief	Jason	Henry		Chippewas of Kettle and Stony Point First Nation		6247 Indian Lane			ON		jason.henry@kettlepoint.org

Thorndale Bridge Improvements
Municipal Class Environmental Assessment, County of Middlesex
Property Owners Contact List

First Name	Secondary Owner	Mailing Address	MailingA_1	City/Prov/Country	Postal Code	Email
THORNDALE STORAGE CITY LTD		[redacted]	21473 NISSOURI RD	THORNDALE ON	N0M 2P0	[redacted]
[redacted]	[redacted]	[redacted]	[redacted]	THORNDALE ON	[redacted]	
[redacted]	[redacted]	[redacted]		THORNDALE ON	[redacted]	
[redacted]	[redacted]	[redacted]	[redacted]	THORNDALE ON	[redacted]	
[redacted]	[redacted]	[redacted]		THORNDALE ON	[redacted]	
[redacted]	[redacted]	[redacted]	[redacted]	THORNDALE ON	[redacted]	
CONSERVATION AUTHORITY UPPER		1424 CLARKE RD		LONDON ON	N5V 5B9	
[redacted]	[redacted]	[redacted]		THORNDALE ON	[redacted]	
[redacted]	[redacted]	[redacted]		THORNDALE ON	[redacted]	
THAMES CENTRE MUNICIPALITY		4305 HAMILTON RD		DORCHESTER ON	N0L 1G3	

Obtained from C.Traini on March 26, 2019 from Assessment Roll

Project No. 165001122

Thorndale Bridge Improvements
Municipal Class Environmental Assessment, County of Middlesex
Public Contact List

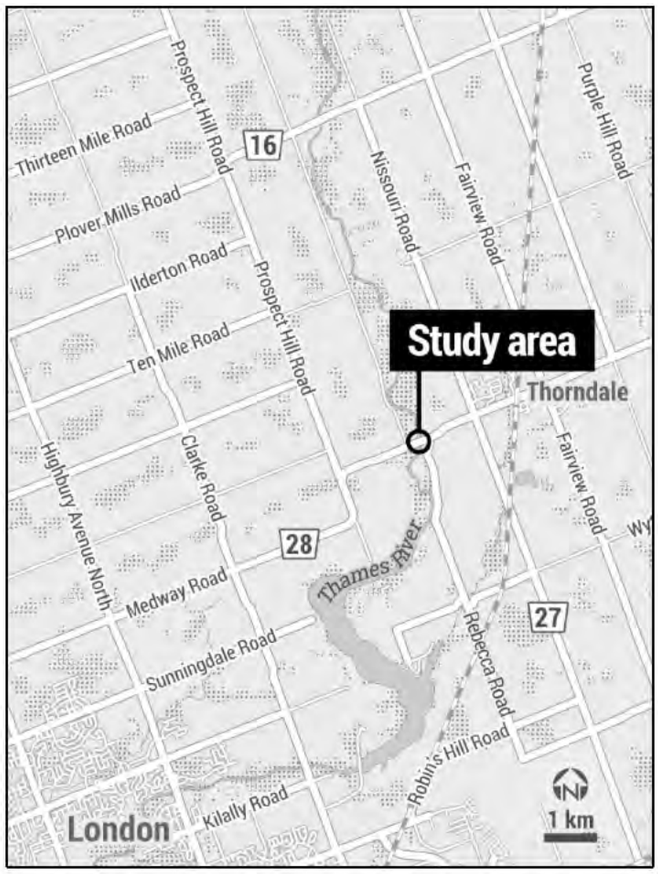
Title	First	Last	Title	Business	Address1	Address2	City	Prov	Postal_Code	Email
Public Contact List										
[redacted]	[redacted]	[redacted]	[redacted]	Bella Vita Aesthetics	237 King St		Thorndale	ON	N0M 2P0	[redacted]
	[redacted]	[redacted]		CSC Blinds West	22 Moneteith Ave		Thorndale	ON	N0M 2P0	
				Grey Lane Kennels	16955 Wyton Drive		Thorndale	ON	N0M 2P0	[redacted]
	[redacted]	[redacted]	[redacted]	Hair By Melissa	22135 Purple Hill Road		Thorndale	ON	N0M 2P0	
	[redacted]	[redacted]	[redacted]	Heeman's	20422 Nissouri Road		Thorndale	ON	N0M 2P0	
[redacted]	[redacted]	[redacted]		Forest City Archers	[redacted]		London	ON	[redacted]	forestcityarchers.info@gmail.com
	[redacted]	[redacted]	[redacted]	Fanshawe Pioneer Villiage	2609 Fanshae Park Road East		London	ON	N5X 4A1	director@fanshawepioneervillage.ca
				London Rowing Club	PO Box 67012 Sunningdale PO		London	ON	N6G 0V5	lrc@londonrowingclub.on.ca
				Row Ontario	227 - 19 Waterman Ave		Toronto	ON	M4B 1Y2	rowontarioadmin@rowontario.ca
				Fanshawe Yacht Club	PO Box 32041, RPO Northland		London	ON	N5V 5K4	
				YMCA Children's Safety Village/ YMCA Camps	1424 Clarke Road		London	ON	N5V 5B9	outdoored@swo.ymca.ca
				Kinsmen Club of Greater London	21201-c Lakeside Drive		Thames Centre	ON	N0M 2P0	
	[redacted]	[redacted]		Thorndale Storage	21473 Nissouri Road		Thorndale	ON	N0M 2P0	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]		London	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]	[redacted]	Thorndale	ON	[redacted]	[redacted]
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]	[redacted]	Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]	[redacted]	Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]	[redacted]	Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]	[redacted]	Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Arva	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]	[redacted]	[redacted]						
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
[redacted]	[redacted]	[redacted]			[redacted]		Thorndale	ON	[redacted]	
	[redacted]	[redacted]			[redacted]	[redacted]	Arva	ON	[redacted]	[redacted]

APPENDIX A.2

Notice of Study Commencement

NOTICE OF STUDY COMMENCEMENT
Thorndale Bridge Improvements
Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road).



The existing bridge is approximately 65 years old and has been identified for replacement within the next 10 years. To establish the most appropriate solution to this need, the study will consider alternative solutions, including: do nothing (retain the existing bridge as is), rehabilitation or replacement of the Thorndale Bridge. Alternatives will be evaluated based on a range of factors and criteria.

The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (October 2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*.

The Class EA study will include:

- Public, agency, and Indigenous community consultation;
- Inventory of existing natural, socio-economic, and cultural environments;
- Analysis of existing and future travel needs;
- Development and evaluation of alternative solutions and alternative designs; and
- Review of potential environmental impacts and proposed mitigation.

Your comments and questions are encouraged throughout the study. Two Public Information Centres (PICs) will be held throughout the course of the study to provide an opportunity to review and discuss the study findings and recommendations with the study team. Subsequent notices will include the date, time, and location for PICs.

To be added to the study mailing list or provide comment, please contact a member of the study team below:

Chris Traini, P.Eng.
Project Engineer
County of Middlesex
ctraini@middlesex.ca
519-434-7321 ext. 2347

Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Personal information collected on this subject is collected under the authority of the *Environmental Assessment Act* and the *Municipal Freedom of Information and Protection of Privacy Act* for transparency and consultation purposes. With the exception of personal information, comments and information received will be maintained on file for use during the study and may be included in project documentation.

This Notice was issued on April 8, 2019.

File # 165001122
Labels for Agencies
04/08/2019

Project Evaluator, Project Review Unit - Ministry of Environment, Conservation and Parks
135 St. Clair Ave West 1st Floor Toronto ON M4V 1Ps

Director - Ministry of Environment, Conservation and Parks
135 St. Clair Ave West 1st Floor
Toronto ON M4V 1Ps

Paul Santos Senior Project Manager
Ministry of Transportation
659 Exeter Road 3rd Floor
London ON N6E 1L3

Heritage Planner
Ministry of Tourism, Culture and Sport
401 Bay Street
Toronto ON M7A 0A7

Kathy Bunting County Clerk
Middlesex County
399 Ridout Street North
London ON N6A 2P1

Chris Traini County Engineer
Middlesex County
399 Ridout Street North
London ON N6A 2P1

Cathy Saunders City Clerk
City of London
300 Dufferin Avenue PO Box 5035
London ON N6A 4L9

Al Hunt Deputy Chief, Operations
Middlesex-London EMS Service
340 Waterloo Street
London ON N6B 2N6

Thorndale Fire Department
17198 Thorndale Rod
Thorndale ON N0M 2P0

Ontario Provincial Police
823 Exeter Road
London ON N6E 1W1

Laura Elliott Director of Education
Thames Valley District School Board
1250 Dundas Street
London ON N5W 5P2

Linda Staudt Director of Education
London Catholic District School Board
5200 Wellington Road South
London On N6E 3X8

Thorndale Community Centre
265 Upper Queen Street
Thorndale ON N0M 2P0

Heather Paton
South Nissouris Presbyterian Church
17358 Oliver Road
Thorndale ON

Stantec Consulting Ltd.
600-171 Queens Avenue, London ON N6A 5J7

April 8, 2019
File: 165001122

Reference: Notice of Study Commencement
Thorndale Bridge Improvements
Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road).

The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference.

The purpose of this letter is to introduce the project, to seek your input on the existing conditions within the study area, and to identify any issues or concerns that you may have.

We respectfully request that you **complete and return the enclosed form by April 26, 2019** so that your comments may be considered early in the Class EA process. If you have any questions regarding the project, please contact either the undersigned or one of the individuals named in the enclosed material.

Regards,

Stantec Consulting Ltd.

Isaac Bartlett P.Eng
Project Manager
Phone: 519-675-6643
Fax: 519-645-6575
isaac.bartlett@stantec.com

Attachment: Notice of Study Commencement, Comment Form

c. Chris Traini, Project Engineer – County of Middlesex
Paula Burnard, Senior Environmental Planner – Stantec Consulting Ltd.

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY
Thorndale Bridge Improvements
Middlesex County

COMMENT FORM – PLEASE RETURN BY APRIL 26, 2019

Date: _____

☐ Please remove this agency from the study mailing list.

☐ Please consider the following input during the Class EA study (see over for additional space).

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

Please return the completed form to:
Isaac Bartlett, P.Eng.
Project Manager, Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Key Project Contact:		
Job Title:		
Name of Group/Agency:		
Mailing Address:		
Tel:	Fax:	E-mail:

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY
Thorndale Bridge Improvements
Middlesex County

COMMENT FORM – PLEASE RETURN BY APRIL 26, 2019

File # 165001122
Labels for Indigenous contacts
04/08/2019

Chief Henry Myeengun
Chippewas of the Thames
320 Chippewa Road RR1
Muncey ON N0L 1Y0

Ms. Rochelle Smith
Consultation Coordinator - Chippewas of the Thames
320 Chippewa Rd RR1 Muncey ON N0L 1Y0

Chief Jessica Hill
Oneida Nation of the Thames
2210 Elm Avenue
Southwold ON N0L 2G0

Ms. Cherilyn Hill - Political Office Manager
Oneida Nation of the Thames
2210 Elm Avenue
Southwold ON N0L 2G0

Chief Roger Thomas
Munsee-Delaware Nation
289 Jubilee Road
Muncey ON N0L 1Y0

Mr. Glenn Forrest - Director of Operations
Munsee-Delaware Nation
289 Jubilee Road
Muncey ON N0L 1Y0

Chief Denise Stonefish
Delaware Nation (Moravian of the Thames)
14760 School House Line RR #3
Thamesville ON N0P 2K0

Consultation Assistant
Delaware Nation (Moravian of the Thames)
14760 School House Line
Thamesville ON N0P 2K0

Chief Dan Miskokomon
Bkejwanong Territory (Walpole Island)
117 Tahgahoning Road RR #3
Walpole Island ON N8A 4K9

Ms. Janet Macbeth - Project Review Coordinator
Bkejwanong Territory (Walpole Island)
117 Tahgahoning Road RR #3 Walpole Island ON N8A 4K9

Ms. Nikki Orosz - Director of Operations
Caldwell First Nation
14 Orange Street P.O.Box 388
Leamington ON N8H 1P5

Chief Chris Plain
Aamjiwnaang First Nation
978 Tashmoo Avenue,
Sarnia ON N7T 7H5

Chief Jason Henry
Chippewas of Kettle and Stony Point First Nation
6247 Indian Lane Lambton Shores N0N 1J2

Stantec Consulting Ltd.
600-171 Queens Avenue, London ON N6A 5J7

April 8, 2019
File: 165001122

Attention: Chief Henry Myeengun
Chippewas of the Thames
320 Chippewa Road RR1
Muncey ON N0L 1Y0

Dear Chief Myeengun,

Reference: Notice of Study Commencement - Thorndale Bridge Improvements Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road). The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference. The purpose of this letter is to introduce the project, to seek your input on the existing conditions within the study area, and to identify any issues or concerns that you may have.

In accordance with the Municipal Class EA process, a Stage 1 Archaeological Assessment is being carried out and will be submitted to the Ministry of Tourism, Culture and Sport to confirm that reporting was completed in accordance with the applicable standards.

We respectfully request that you **complete and return the enclosed form by April 26, 2019** so that your comments may be considered early in the Class EA process. If you would prefer a meeting with Middlesex County staff, that could also be arranged. Please contact me at your earliest convenience if you are interested in such a meeting.

Regards,

Stantec Consulting Ltd.

Isaac Bartlett P.Eng
Project Manager
Phone: 519-675-6643
isaac.bartlett @stantec.com

Enclosure: Notice of Study Commencement
Comment Form

c. Chris Traini, Project Engineer – County of Middlesex
Paula Burnard, Senior Environmental Planner – Stantec Consulting Ltd.

COMMENT FORM

Municipal Class Environmental Assessment Study for Improvements to the Thorndale Bridge, Middlesex County

I have an interest in the project and wish to be added to the study mailing list for the Thorndale Bridge Improvements project. My contact information is provided below.

Name:	911 Property ID #:
Mailing Address:	Postal Code:
	Telephone:
I prefer to be contacted by Mail__ Email:__	Email Address:

Comments:

Please return the completed form by April 29, 2019 to:

Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
600-171 Queens Avenue, London, ON N6A 5J7
Tel. 519-675-6643, Fax 519-645-6575, Email: isaac.bartlett@stantec.com

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

COMMENT FORM

Municipal Class Environmental Assessment Study for Improvements to the Thorndale Bridge, Middlesex County

Additional comments:

Easy Peel® Labels
Use Avery® Template 5160®

Feed Paper

Bend along line to expose Pop-up Edge™

AVERY® 5160®

File # 165001122
Labels for Public contacts
04/08/2019

[redacted]
Bella Vita Aesthetics
237 King St
Thorndale ON N0M 2P0

[redacted]
CSC Blinds West
22 Moneteith Ave
Thorndale ON N0M 2P0

Grey Lane Kennels
16955 Wyton Drive
Thorndale ON N0M 2P0

[redacted]
Hair By Melissa
22135 Purple Hill Road
Thorndale ON N0M 2P0

[redacted]
Heeman's
20422 Nissouri Road
Thorndale ON N0M 2P0

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Stantec Consulting Ltd.
600-171 Queens Avenue, London ON N6A 5J7

April 8, 2019
File: 165001122

Reference: Notice of Study Commencement
Thorndale Bridge Improvements
Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road).

The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference.

The purpose of this letter is to introduce the project, to seek your input on the existing conditions within the study area, and to identify any issues or concerns that you may have.

We respectfully request that you **complete and return the enclosed form by April 26, 2019** so that your comments may be considered early in the Class EA process. If you have any questions regarding the project, please contact either the undersigned or one of the individuals named in the enclosed material.

Regards,

Stantec Consulting Ltd.

Isaac Bartlett P.Eng
Project Manager
Phone: 519-675-6643
Fax: 519-645-6575
isaac.bartlett@stantec.com

Attachment: Notice of Study Commencement, Comment Form

c. Chris Traini, Project Engineer – County of Middlesex
Paula Burnard, Senior Environmental Planner – Stantec Consulting Ltd.

COMMENT FORM

Municipal Class Environmental Assessment Study for Improvements to the Thorndale Bridge, Middlesex County

I have an interest in the project and wish to be added to the study mailing list for the Thorndale Bridge Improvements project. My contact information is provided below.

Name:	911 Property ID #:
Mailing Address:	Postal Code:
	Telephone:
I prefer to be contacted by Mail__ Email:__	Email Address:

Comments:

Please return the completed form by April 29, 2019 to:

Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
600-171 Queens Avenue, London, ON N6A 5J7
Tel. 519-675-6643, Fax 519-645-6575, Email: isaac.bartlett@stantec.com

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

COMMENT FORM

Municipal Class Environmental Assessment Study for Improvements to the Thorndale Bridge, Middlesex County

Additional comments:

File # 165001122
Labels for Property Owners
04/08/2019

THORNDALE STORAGE CITY LTD
[redacted]
[redacted] THORNDALE ON
[redacted]
[redacted]
THORNDALE ON [redacted]

[redacted] E
[redacted]
THORNDALE ON [redacted]
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THORNDALE ON [redacted]
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THORNDALE ON [redacted]

[redacted]
THORNDALE ON [redacted]
[redacted]
[redacted]
THORNDALE ON [redacted]

CONSERVATION AUTHORITY UPPER &
1424 CLARKE RD
LONDON ON N5V 5B9

THAMES CENTRE MUNICIPALITY &
4305 HAMILTON RD
DORCHESTER ON N0L 1G3

Stantec Consulting Ltd.
600-171 Queens Avenue, London ON N6A 5J7

April 8, 2019
File: 165001122

[redacted]
THORNDALE ON [redacted]

Dear [redacted]

Reference: NOTICE OF STUDY COMMENCEMENT AND PERMISSION TO ENTER, Middlesex County, Thorndale Bridge Improvements, Municipal Class Environmental Assessment

Middlesex County is initiating a Municipal Class Environmental Assessment (Class EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road). The Class EA will consider alternative solutions to the replacement of the bridge to address existing and future travel needs, while assessing natural environment impacts, infrastructure needs, property impacts, and costs. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference.

To gather necessary information, Middlesex County requests your permission to enter your property located within the study area. The purpose of the permission to enter is to conduct field surveys of natural features (trees, plants, birds and bird nesting, etc.) in close proximity and adjacent to the bridge, right-of-way of County Road 28 and along the Thames River. The field investigations required will assist the project team to document existing natural environment conditions, identify potential impacts during construction and to develop mitigation measures to be incorporated into the design of the bridge replacement project. The field investigations are not anticipated to have any impact on your property.

These investigations will be carried out by different teams and will likely occur on different days during the spring, summer and fall of this year.

Please sign and date the enclosed Consent to Enter Form and return to our office by no later than April 12, 2019. A return postage paid envelope is enclosed for this purpose. Alternatively, you can email the form directly to me at paula.burnard@stantec.com.

If you have any questions, concerns, or want clarification about this information, please feel free to contact the undersigned or other members of our study team:

Chris Traini, Project Engineer
County of Middlesex
519-434-7321 ext. 2347
ctraini@middlesex.ca

Isaac Bartlett, Project Manager
Stantec Consulting Ltd.
519-675-6643
Isaac.bartlett@stantec.com

Reference: NOTICE OF STUDY COMMENCEMENT AND PERMISSION TO ENTER, Middlesex County, Thorndale Bridge Improvements, Municipal Class Environmental Assessment

Regards,

Stantec Consulting Ltd.

Isaac Bartlett P.Eng.
Project Manager, Transportation
Phone: 519 675 6643
Fax: 519 645 6575
isaac.bartlett@stantec.com

Attachment: Permission to Enter Form
Return postage paid envelope
Notice of Study Commencement
Comment Form

c. Paula Burnard, Senior Environmental Planner – Stantec Consulting Ltd.
Chris Traini, Project Engineer – County of Middlesex

Stantec Consulting Ltd.
600-171 Queens Avenue, London ON N6A 5J7

April 8, 2019
File: 165001122

Reference: NOTICE OF STUDY COMMENCEMENT AND PERMISSION TO ENTER, Middlesex County, Thorndale Bridge Improvements, Municipal Class Environmental Assessment

TO: PROPERTY OWNER

FOR VALUABLE CONSIDERATION, the receipt and sufficiency of which is hereby acknowledged, the undersigned being the owner(s) of _____, in Thorndale (the "Property"), hereby consent to the entry onto the Property by the County and its authorized representatives and consultants with the necessary equipment for the purpose of investigative activities related to the initiation of the Thorndale Bridge Improvements Municipal Class Environmental Assesssment.

PROVIDED THAT any entry onto the Property shall be at the County's sole risk and expense and provided that the County shall restore the Property to the same condition that existed before such entry.

AND PROVIDED THAT the County shall indemnify and save the undersigned harmless from any and all liability, loss, claims, demands, and costs occasioned by the right of entry granted herein.

DATED at _____, in the _____ this _____ day of _____ 2019.
(City) (Municipality)

_____ _____
(Signature of Owner) (Print Name of Owner)

_____ _____
(Signature of Witness) (Print Name of Witness)

Please sign and return the form to the address below by Friday, April 12, 2019.

Isaac Bartlett, P.Eng., Project Manager
Stantec Consulting Ltd.
600-171 Queens Avenue,
London, ON N6A 5J7
isaac.bartlett@stantec.com

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY
Thorndale Bridge Improvements
Middlesex County

COMMENT FORM – PLEASE RETURN BY APRIL 26, 2019

Date: _____

☐ Please remove this agency from the study mailing list.

☐ Please consider the following input during the Class EA study (see over for additional space).

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

Please return the completed form to:
Isaac Bartlett, P.Eng.
Project Manager, Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Key Project Contact:		
Job Title:		
Name of Group/Agency:		
Mailing Address:		
Tel:	Fax:	E-mail:

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY
Thorndale Bridge Improvements
Middlesex County

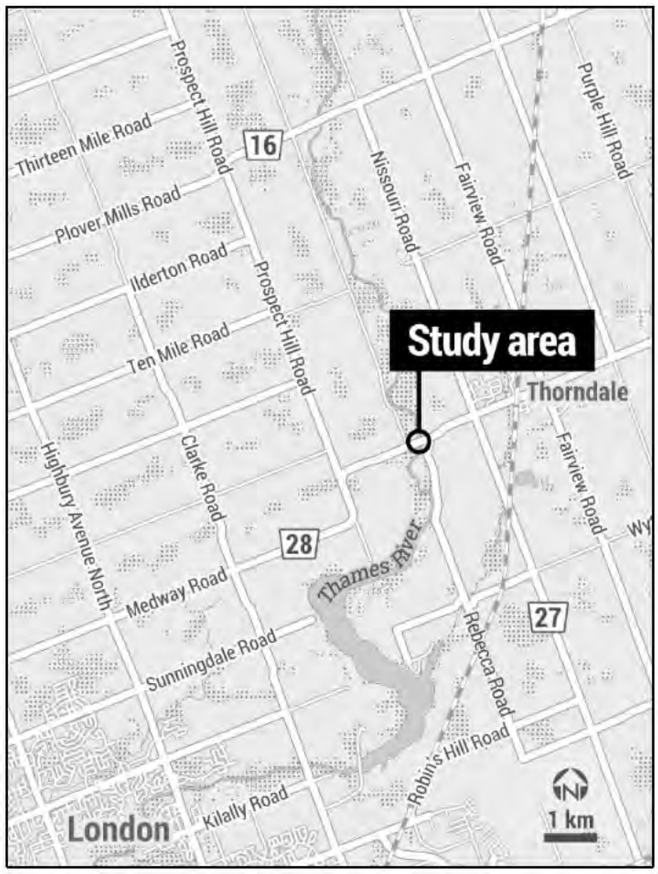
COMMENT FORM – PLEASE RETURN BY APRIL 26, 2019

APPENDIX A.3

Public Information Centre 1

NOTICE OF PUBLIC INFORMATION CENTRE #1
Thorndale Bridge Improvements
Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road).



The existing bridge is approximately 65 years old and has been identified for replacement within the next 10 years. To establish the most appropriate solution to this need, the study will consider alternative solutions, including: do nothing (retain the existing bridge as is), rehabilitation or replacement of the Thorndale Bridge. Alternatives will be evaluated based on a range of factors and criteria.

The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (October 2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*.

A key component of the study will be consultation with interested stakeholders (public and regulatory agencies) through Public Information Centres (PIC). The first of two (2) Public Information Centres will be held on:

Date:	Wednesday, September 25, 2019
Time:	Drop-in between 5:00 – 7:00 p.m.
Location:	Thorndale Community Centre 265 Queen St., Thorndale ON

The purpose of the PIC is to review and obtain public input on the problem being addressed, background information and the planning alternatives being considered. Anyone with an interest in the study is invited to attend and participate.

If you are unable to attend the Public Information Centre and would like to provide comments, please forward them by Wednesday October 16, 2019 to either Project Team member.

Chris Traini, P.Eng.
Project Engineer
County of Middlesex
ctraini@middlesex.ca
519-434-7321 ext. 2264

Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Personal information collected on this subject is collected under the authority of the *Environmental Assessment Act* and the *Municipal Freedom of Information and Protection of Privacy Act* for transparency and consultation purposes. With the exception of personal information, comments and information received will be maintained on file for use during the study and may be included in project documentation.

This Notice was issued on September 12, 2019.

Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Welcome

Thank you for attending the Public Information Centre (PIC) for the **Thorndale Bridge Improvements Municipal Class Environmental Assessment**

THE PURPOSE OF TONIGHT'S PIC:

The purpose of the PIC is to review and obtain public input on the **problem being addressed**, **background** information and the preliminary assessment of **alternative solutions** being considered.

Anyone with an interest in the study is invited to attend and participate.

WE NEED YOUR INPUT ON:

- The **problem/opportunity statement**
- The **alternative solutions** and **evaluation criteria** used to assess the potential impacts
- The **recommended alternative solution**
- Any **additional information** you would like us to consider and/or incorporate into the study

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

What Is The EA Process?

The Municipal Class EA is an approved process under the Ontario Environmental Assessment Act which municipalities follow for the planning and design of municipal infrastructure projects. The process:

- Identifies needs, problems and opportunities
- Considers a range of reasonable solutions
- Requires public, agency, and Indigenous community consultation
- Documents the decision-making process in a clear and transparent manner.

This study is being planned as a Schedule 'C' project, which involves the completion of Phases 1 through 4 of the planning process.

Phase 1
Problem or Opportunity
← Identify and describe the problem and/or opportunity.

▼

Phase 2
Alternative Solutions
We are here ★
← Prepare environmental inventory, identify/assess alternative solutions and seek public and agency comments.

▼

Phase 3
Alternative Designs for Preferred Solution
← Develop and assess alternative designs, identify potential environmental impacts and proposed mitigation measures.

▼

Phase 4
Environmental Study Report
← Prepare and file an Environmental Study Report (ESR) for a minimum 30 calendar day period to allow for public and agency review.

▼

Phase 5
Implementation
← Construct project as described in the ESR (subject to future municipal capital budgets).

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Study Area

Study Area

- The study area includes the Thorndale Road bridge, located on Thorndale Road (County Road 28), east of Valleyview Road, west of Rebecca Road and approximately120 m north and south of the bridge.
- Thorndale Road is an east-west arterial road that provides connectivity between the communities of Thorndale, Ballymonte and Arva.

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Thorndale Bridge Improvements

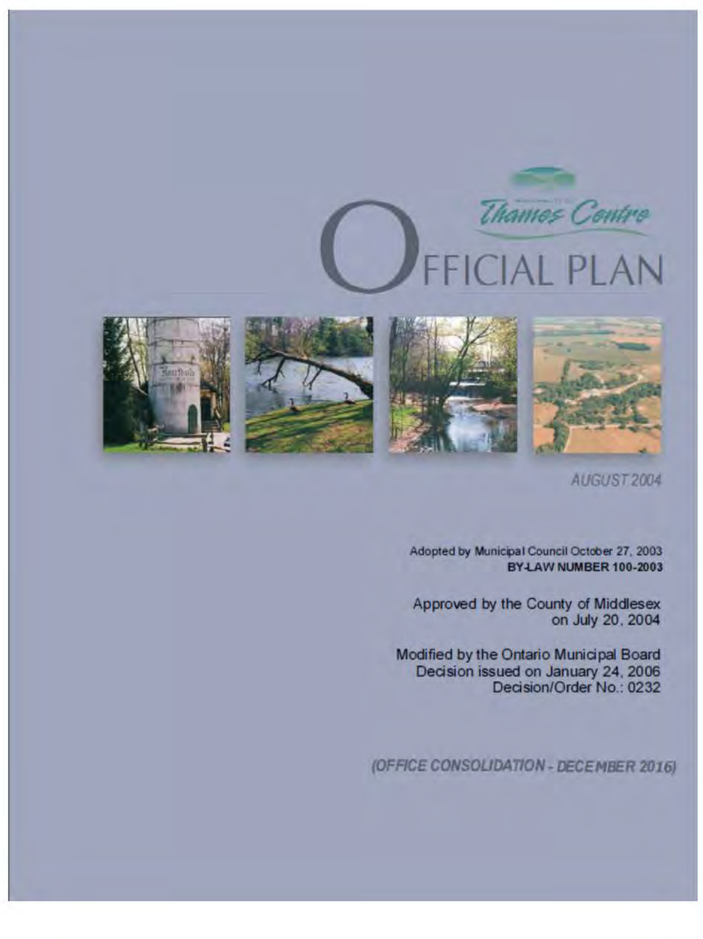
Municipal Class Environmental Assessment

Background Information and Related Studies

A number of documents and studies have been reviewed to gain an understanding of the existing conditions for the study area, including:

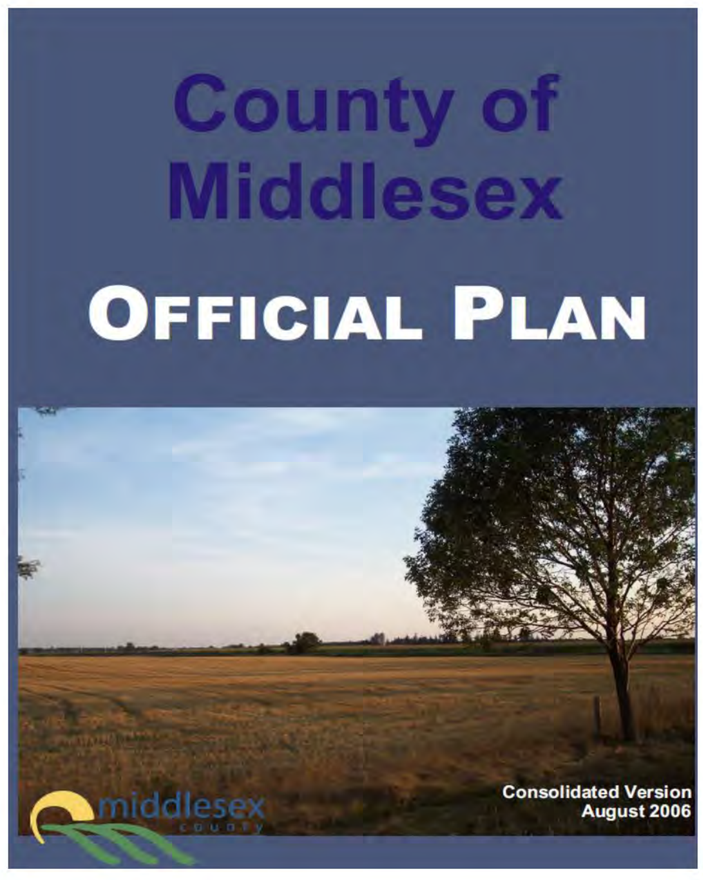
Municipality of Thames Centre Official Plan (2004)

- Promotes safe, convenient, and attractive transportation options for pedestrians and cyclists within, and where feasible between settlement areas.
- Upholds County policies with respect to design and right of way widths, as well as limited property access.
- The bridge is immediately west of the Thorndale Urban Settlement Area – one of two growth areas in the municipality.
- Promotes preserving natural heritage features, such as the Thames River.



County of Middlesex Official Plan (2006)

- Thorndale Road is a County Arterial Road (County Road). County roads provide for the efficient movement of traffic between provincial freeways/highways and local roads.
- Provides direction on County road widths and features.
- Discourages development which would inhibit traffic movement, while encouraging a safe and efficient road network.
- Provides standards for county arterial roads such as right of way widths. Desired right of way widths are 36 m.



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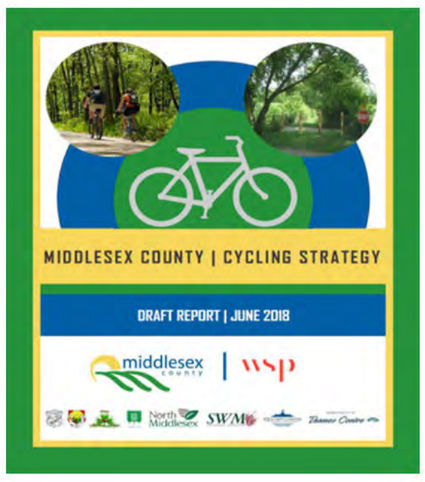
Thorndale Bridge Improvements

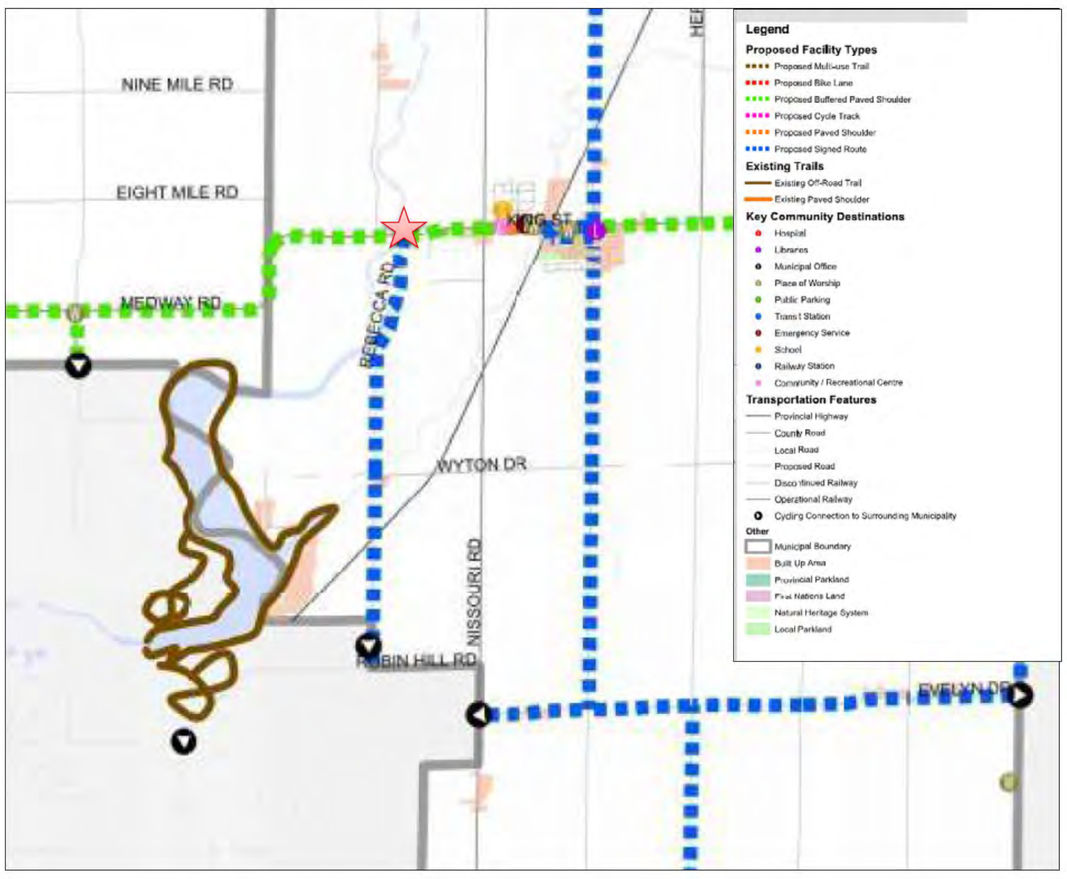
Municipal Class Environmental Assessment

Cycling and Trails

Middlesex County Cycling Strategy (2018)



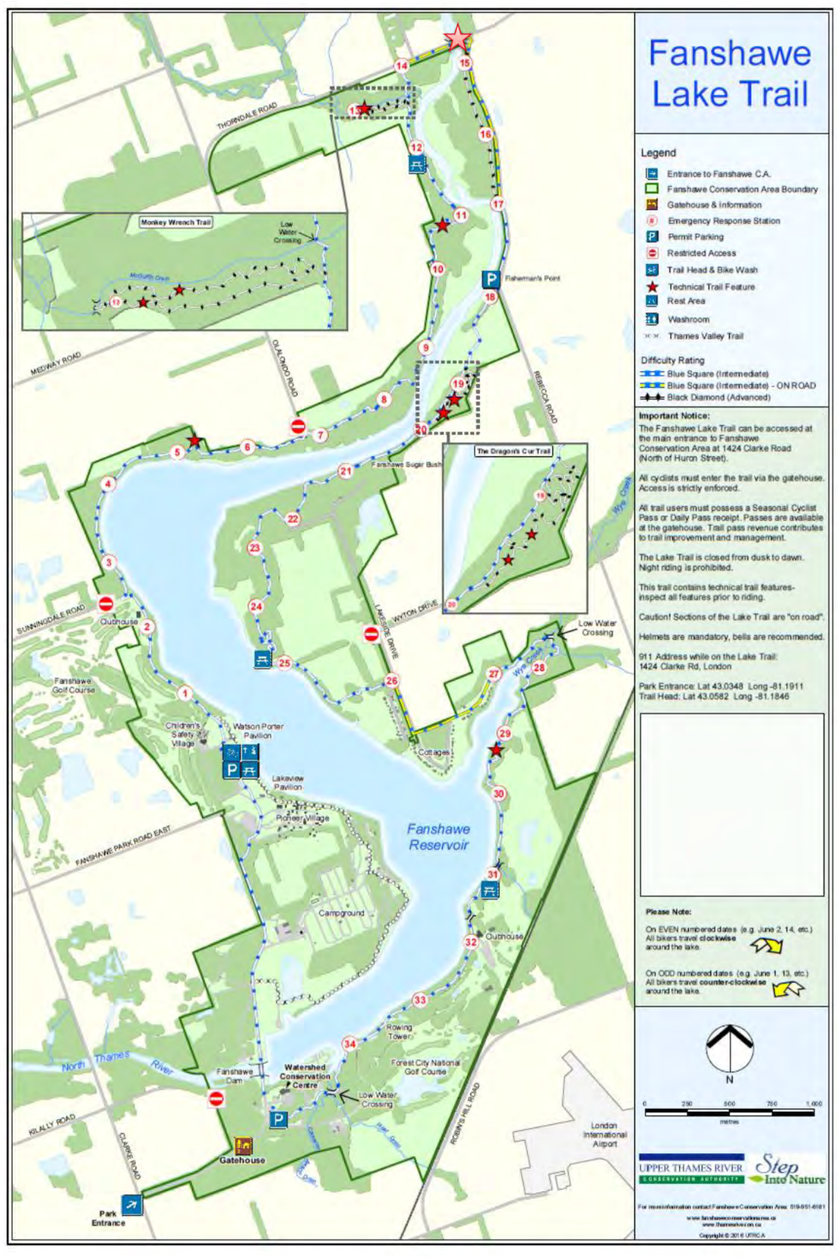
- Identifies cycling routes within the proposed network.
- Buffered paved shoulder (Thorndale Road).
- Proposed signed route (Rebecca Road).
- The Thorndale Bridge is included in the "long-term" network.



Excerpt, Draft Cycling Network Types, (Map 3-5) – Middlesex County Cycling Strategy (2018)

Upper Thames River Conservation Authority Trails

- The Thorndale Bridge is included on the Fanshawe Lake Trail and provides the crossing point over the Thames River for the loop trail.



Fanshawe Lake Trail Map (UTRCA, 2019)

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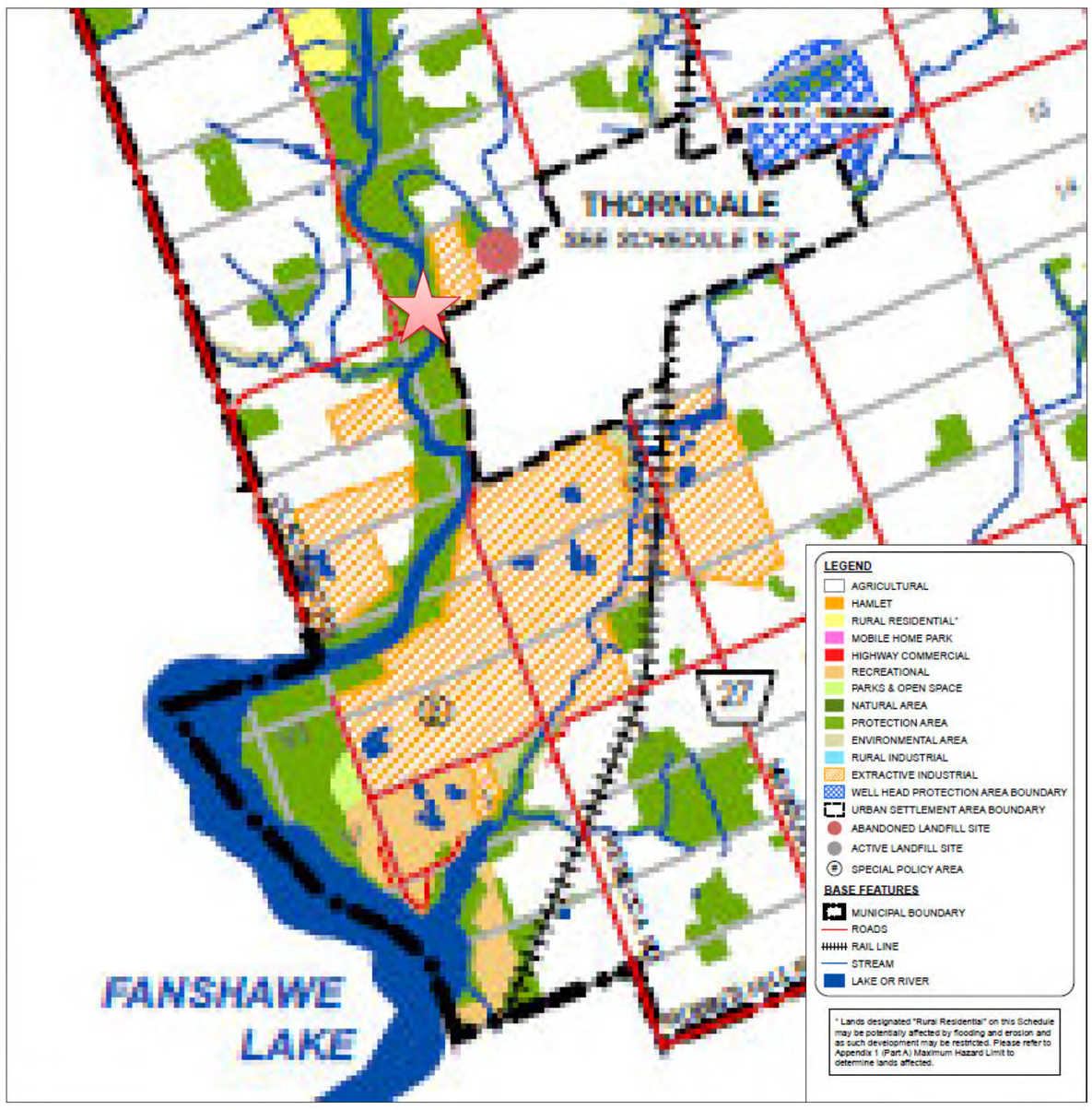
Thorndale Bridge Improvements

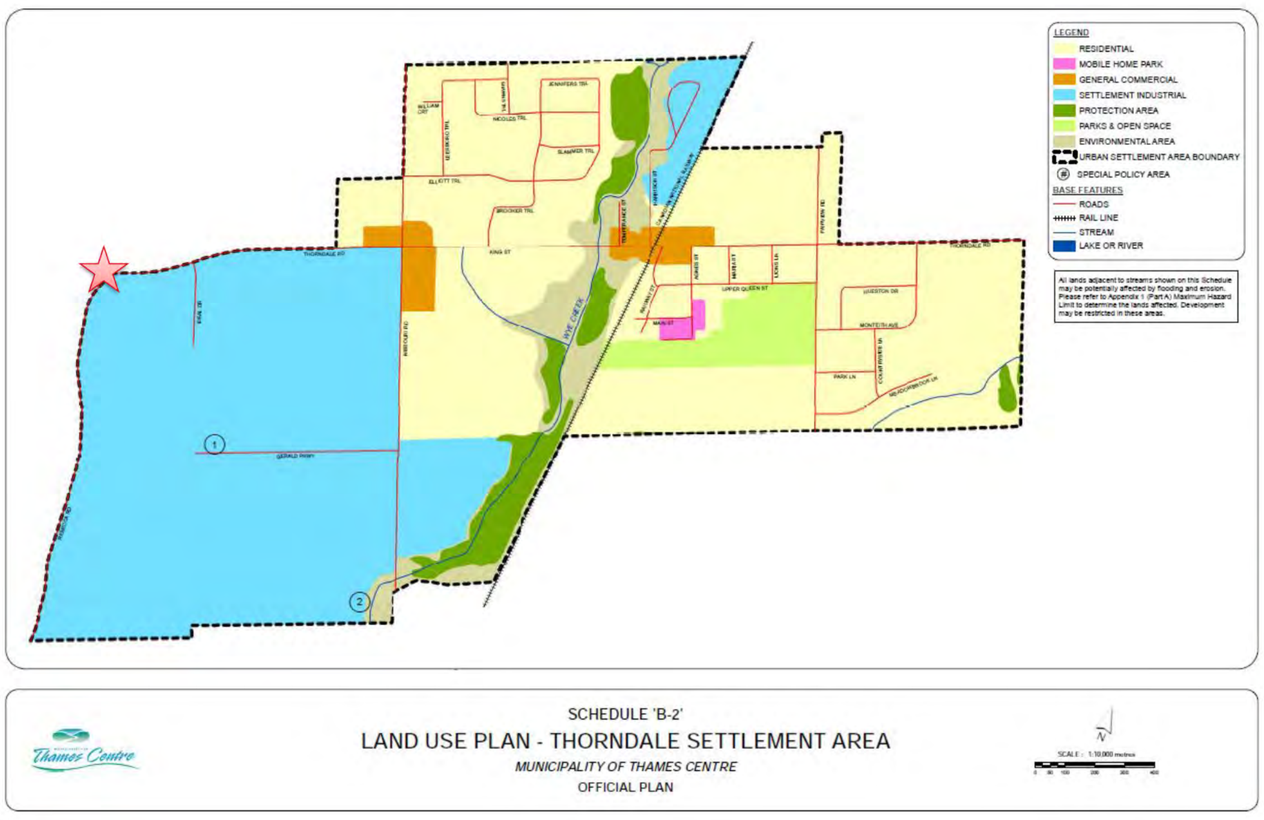
Municipal Class Environmental Assessment

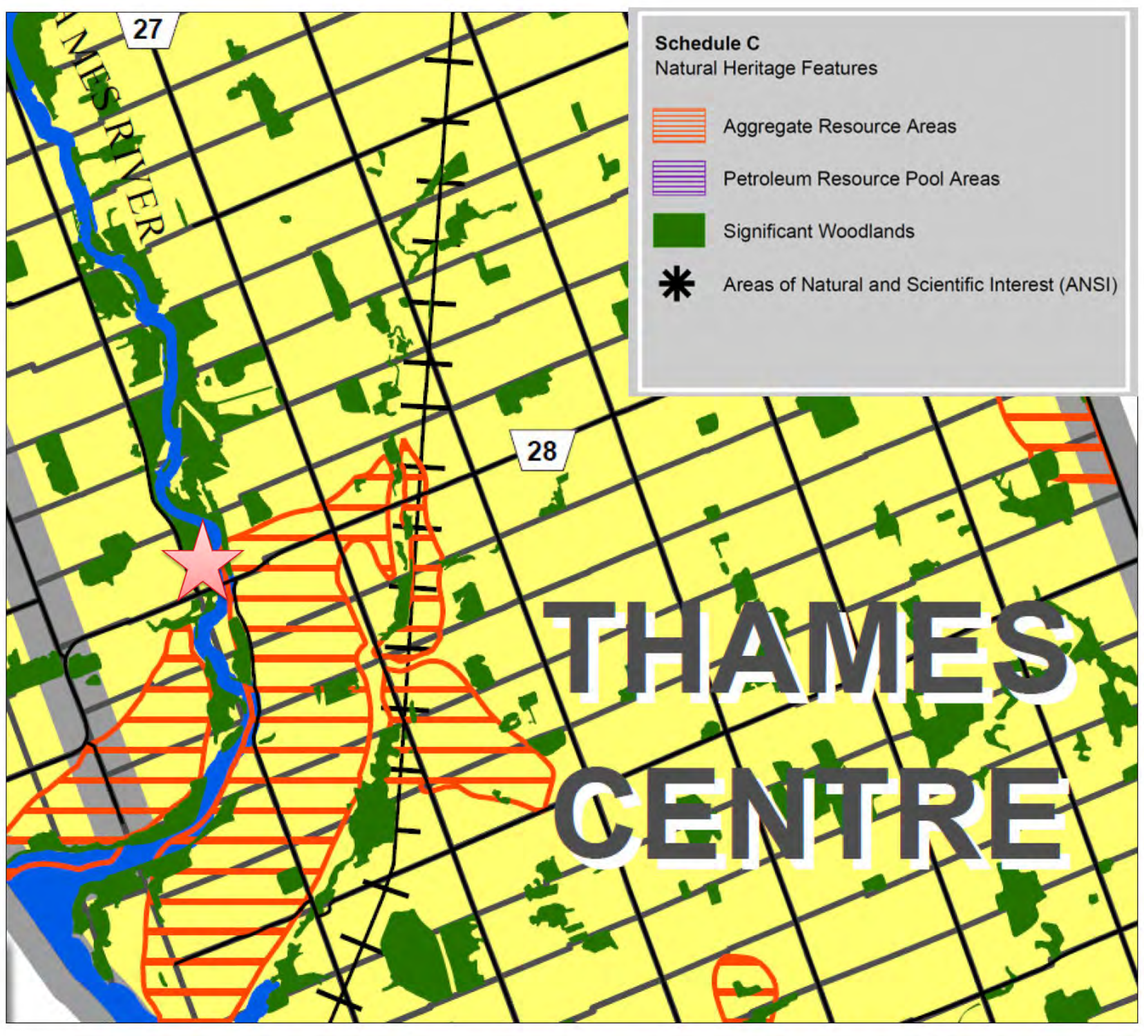
Existing Conditions: Socio-Economic Environment

- Thames River is designated as a Canadian Heritage River and a visual landscape corridor.
- Fanshawe Loop Trail uses the bridge to cross the Thames River.
- Thorndale Road (County Road 28) is an identified truck haul route.
- The bridge is west of the Thorndale Urban Settlement Area.
- Local land use near the site includes natural and protected areas (significant woodland), an abandoned landfill site, and aggregate resource areas.

(see Municipality of Thames Centre, Schedule "A" land use excerpt and Schedule C, Middlesex County Official Plan Natural Heritage Features excerpt)







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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Existing Conditions: Natural Environment

Environmental Impact Study

An Environmental Impact Study (EIS) is being completed to characterize existing natural environmental conditions within the study area. This information will be used to assist with the development and assessment of alternatives, understand potential impacts and the need for mitigation measures.

Terrestrial Ecosystems

- A Tree Inventory & Preservation Plan will be completed

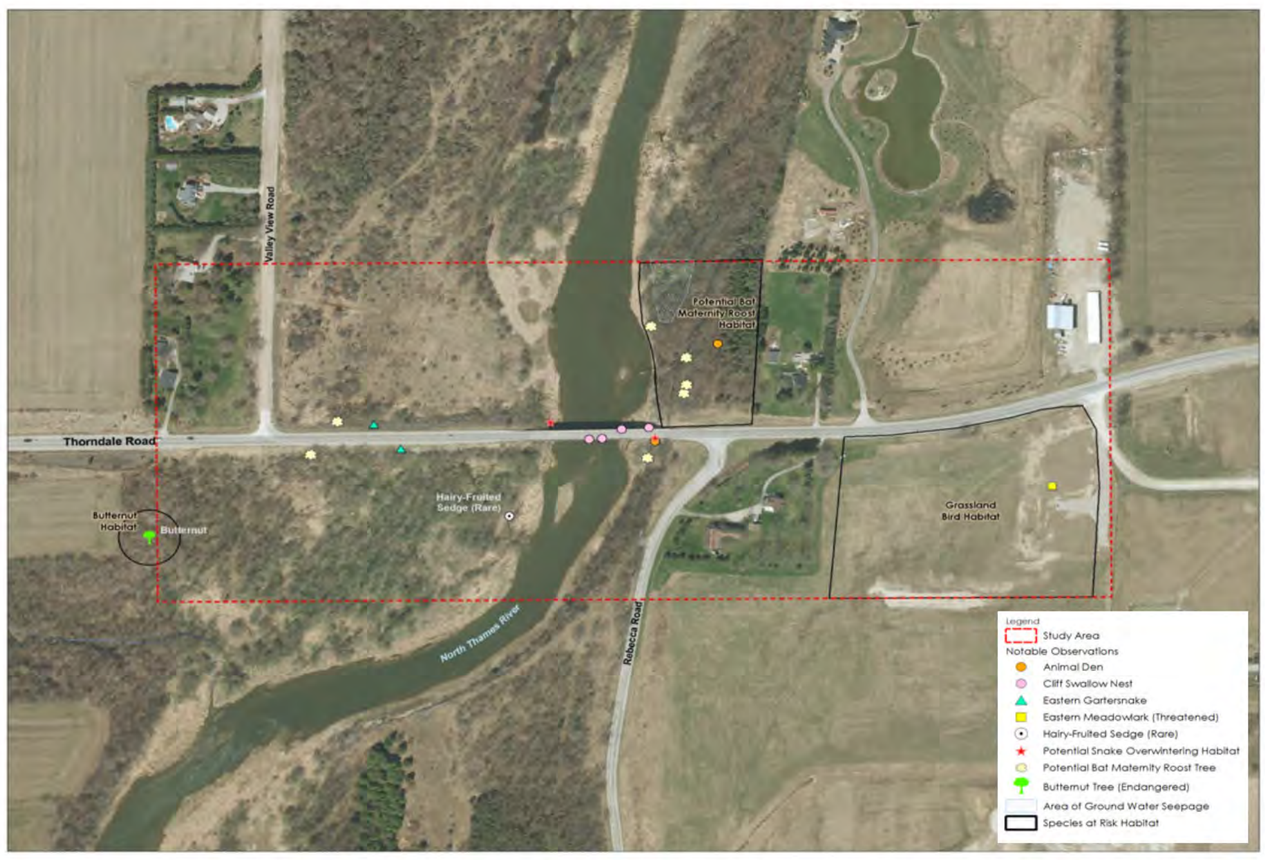
- Significant Wildlife Habitats include:
    - Rare Plants
    - Bat maternity roost habitat
    - Seeps
    - Snake Hibernacula

Fish and Fish Habitat

- Rayed Bean (provincially and federally endangered)
- Wavy-rayed Lampmussel (provincially threatened, federally endangered)
- Rainbow (provincially special concern, federally endangered)

Species at Risk

- Potential habitat for bats (endangered)
- Confirmed habitat for Eastern Meadowlark (threatened) and habitat for Mussels (listed above)
- Confirmed Butternut (endangered) adjacent to study area



Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Existing Conditions: Cultural Environment

Archaeological Resources

- A Stage 1 Archaeological Assessment has been undertaken within the study area to identify areas of archaeological potential and impacts of the bridge improvements.
    - It was determined that much of the study area retains potential for the identification and documentation of archaeological resources.
    - A Stage 2 Archaeological Assessment is required for any portion of the project 's anticipated construction which impacts an area of archaeological potential.

Built Cultural Heritage Resources and Landscapes

- A Cultural Heritage Evaluation Report was completed for the study area to identify potential built cultural heritage resources, and cultural heritage landscapes.
- The Thorndale Bridge is a four-span cast-in-place concrete two-cell box girder structure and was identified as a potential cultural heritage resource (over 40 years old).
- The bridge was determined to have Cultural Heritage Value of Interest (CHVI) specifically for the design/ physical value relating to the box girder structure.
- While this was a common bridge type in the 1950s and 1960s, there are not a lot of bridges remaining in the province.
- Overall, the Thorndale Bridge was determined not to be provincially important and worthy of inclusion on the *Ontario Heritage Bridge List*.

Thorndale Bridge under construction August 28, 1953
(Archives and Special Collections, Western Libraries, Western University 1953)

Thorndale Bridge June 19, 1954
(Archives and Special Collections, Western Libraries, Western University 1954)

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Existing Conditions: Transportation

Thorndale Road (County Road 28) over the Thames River

Thorndale Road within the study area is a **two-lane Arterial Road** with a posted speed limit of **80 km/hr** and serves as a through traffic route with nearly 6,000 vehicles per day.

- Thorndale Bridge was constructed in 1953. Past rehabilitation efforts have extended the life of the structure to a limited degree
- The bridge has four spans over the Thames River, with a current road width of 7.5 m and deck width of 9.5 m.
- Thorndale bridge is noted to have capacity concerns specifically relating to pedestrians/ cyclists and the trail connection around Fanshawe Lake.
- Steep embankments on the approaches are protected by steel beam guiderail.
- Adjacent Hydro One and telecommunication utilities (aerial). No utilities within the structure.
- The intersection of Thorndale Road and Rebecca Road is approximately 40 m from the east end of the structure.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Problem/Opportunity Statement

Based on the review of the existing conditions, municipal priorities, planning and policy documents, and future development, the following summarizes the problems and opportunities within the study area:

Active Transportation

Need to improve active transportation facilities within the study area (buffered paved shoulder identified in Cycling Master Plan) and provide connections to the Fanshawe Lake Trail System.

Bridge Condition

The existing bridge is 67 years old and the County has identified the need for its replacement within the next 10 years.

This study will consider alternatives for replacement or enhancement of the existing structure resulting in improvements for all users. The bridge will provide sufficient road capacity, while safely and efficiently accommodating active transportation.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Alternative Solutions

Improvements to the Thorndale Bridge are required to address the existing bridge condition and to address the opportunity to improve active transportation facilities and connections to the Fanshawe Lake Trail System. The following alternative solutions have been considered:

1 **Do Nothing**
No proposed changes to the bridge. This alternative is included to provide a base to which other alternatives can be compared.

2 **Rehabilitation of Existing Bridge**
Maintain existing bridge and repair sections as required

3 **Replace Superstructure and Detour**
Strengthen existing piers and rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted around bridge construction on detour.

4 **Replace Superstructure and Temporary Modular Bridge (TMB)**
Strengthen existing piers, rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted over TMB on new alignment next to existing bridge

5 **New Bridge and Detour**
Replace the whole bridge on the existing alignment. Traffic rerouted around bridge construction on detour.

6 **New Bridge and Temporary Modular Bridge (TMB)**
Replace the whole bridge on the existing alignment. Traffic rerouted over TMB on new alignment next to existing bridge.

7 **New Bridge on New Alignment**
Replace the whole bridge on a new alignment adjacent to the existing bridge. Traffic maintained on existing bridge during construction.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

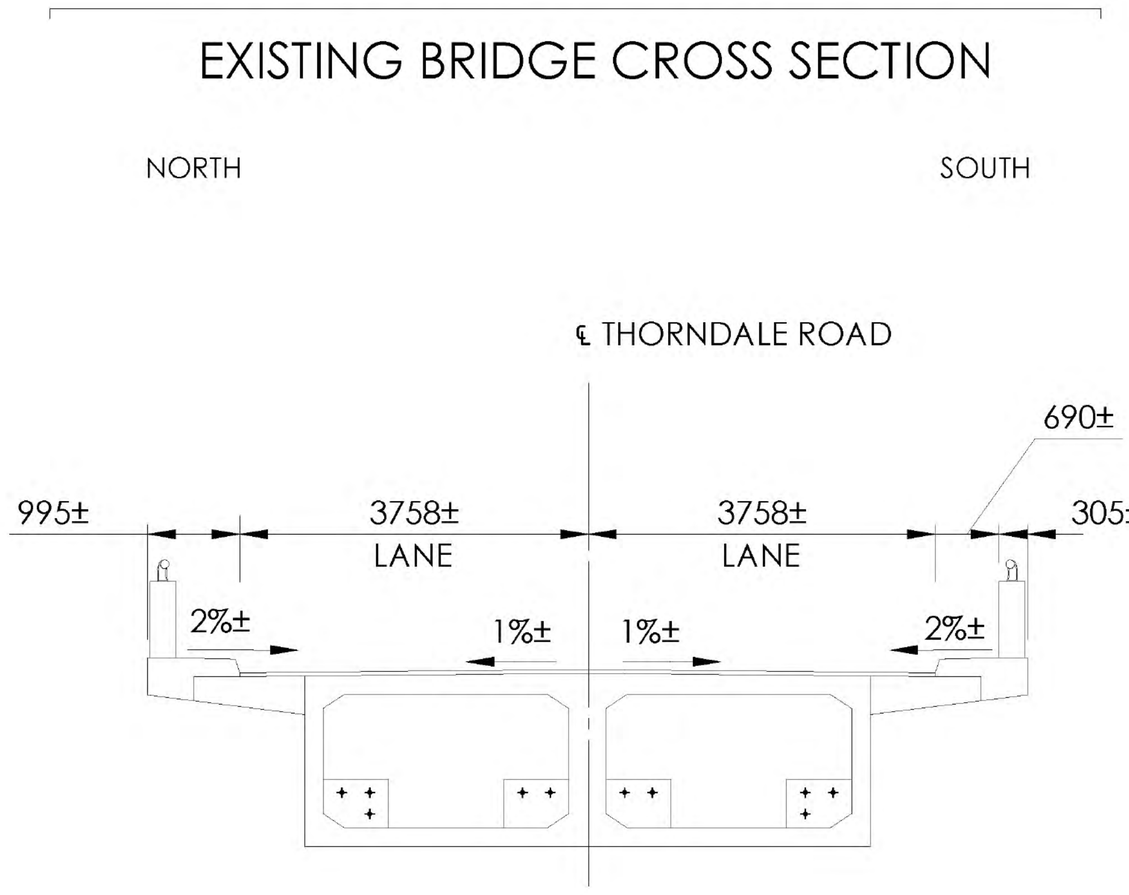
Alternative Solutions

1 Do Nothing

No proposed changes to the bridge. This alternative is included to provide a base to which other alternatives can be compared.

2 Rehabilitation of the Existing Bridge

Maintain existing bridge and repair sections as required.



Pros:

- Lower construction cost in the short term as compared to building new superstructure or bridge
- Low potential impact to wildlife, wildlife habitat and vegetation communities with construction limited to existing structure
- No property impact

Con:

- Increase chance of structure failure and emergency closure of bridge forcing detour route until repairs completed
- Eventual permanent structure closure
- Does not accommodate active transportation facilities with standard shoulders or sidewalk

Stantec

Thorndale Bridge Improvements

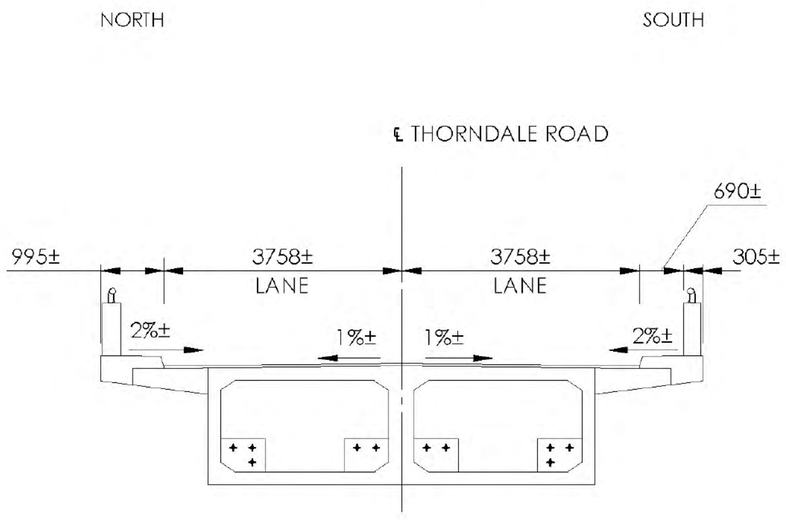
Municipal Class Environmental Assessment

Alternative Solutions

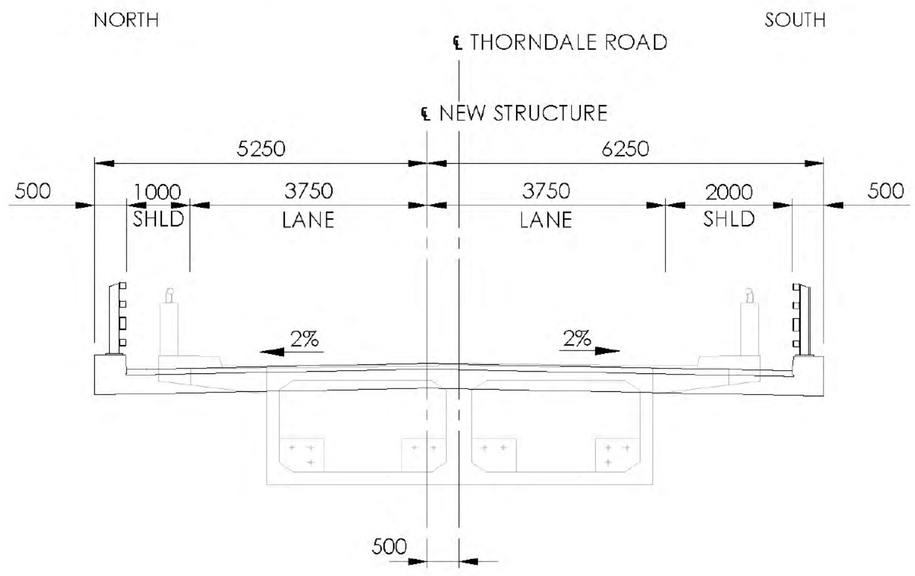
3 Replace Superstructure and Detour

Strengthen existing piers and rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted around bridge construction on detour.

EXISTING BRIDGE CROSS SECTION



REPLACE SUPERSTRUCTURE USING DETOUR



Pros:

- Accommodate active transportation facilities with standard shoulders or sidewalk
- Lower construction and temporary costs due to use of existing piers, and roads for detour
- Relatively lower impact to wildlife, wildlife habitat and vegetation communities (smaller overall impact area)

Con:

- Remaining life of existing piers is shorter than the rest of the structure; pier strengthening required
- Temporary impacts to existing traffic, EMS and trail users for one construction season

Stantec

Thorndale Bridge Improvements

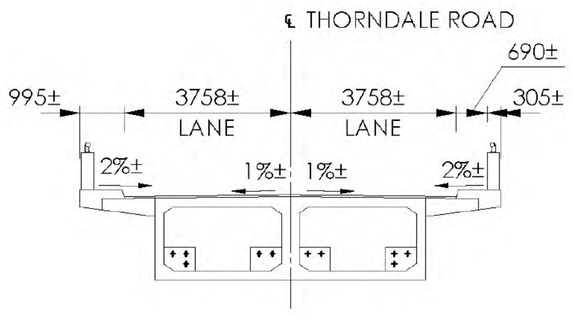
Municipal Class Environmental Assessment

Alternative Solutions

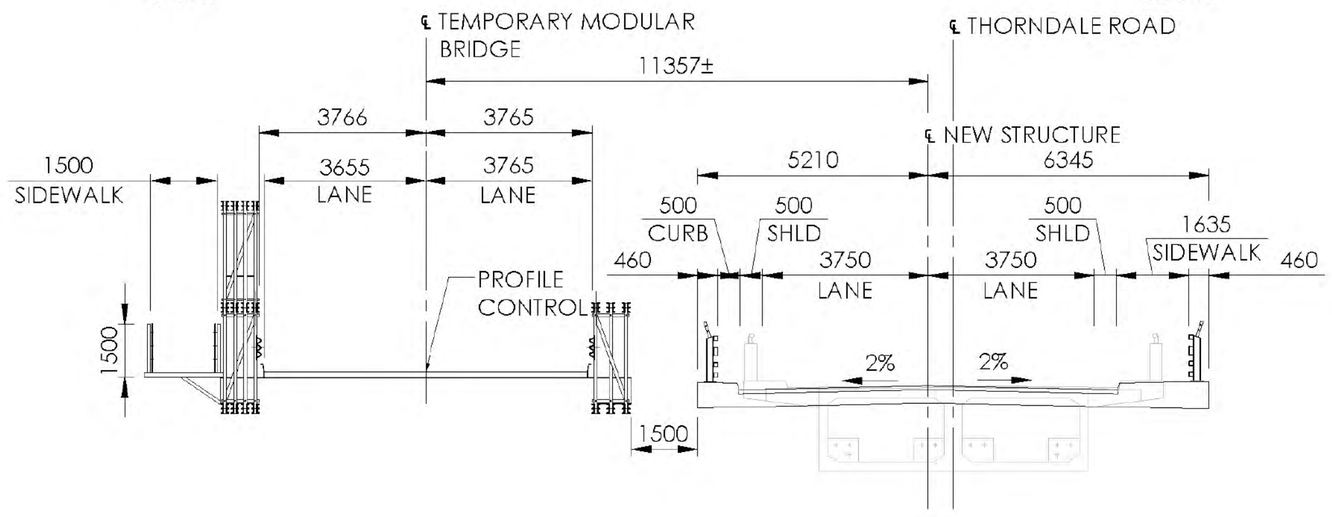
4 Replace Superstructure and Temporary Modular Bridge (TMB)

Strengthen existing piers, rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted over TMB on new alignment next to existing bridge.

EXISTING BRIDGE CROSS SECTION



REPLACE SUPERSTRUCTURE USING TMB



Pros:

- Accommodates active transportation facilities with standard shoulders or sidewalk
- Minimal impact to existing traffic, EMS and trail users due to TMB
- Lower construction cost as compared to building a new bridge

Con:

- Remaining life of existing piers will be shorter than the rest of the structure; pier strengthening required
- High cost for temporary modular bridge as compared to detour route using existing roads
- Higher impact to wildlife, wildlife habitat and vegetation due to TMB and in-water work (larger overall impact area)
- Potential to impact SAR species due to in-water work and embankment alterations
- Temporary property impact due to TMB

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Thorndale Bridge Improvements

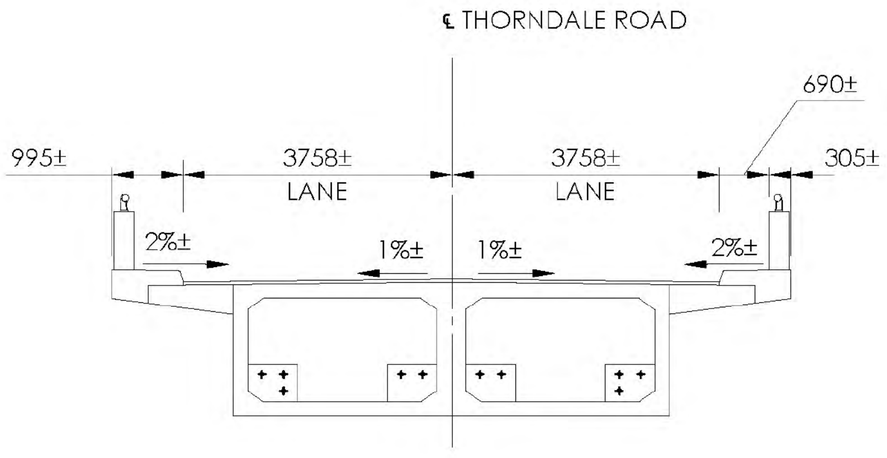
Municipal Class Environmental Assessment

Alternative Solutions

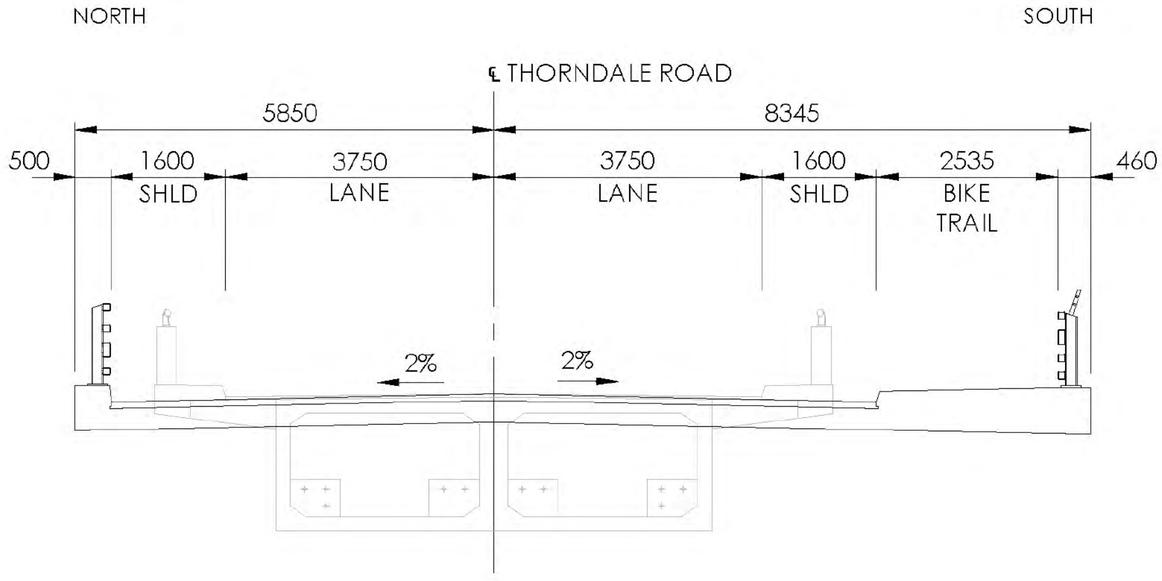
5 New Bridge and Detour

Replace the whole bridge on the existing alignment. Traffic rerouted around bridge construction on detour.

EXISTING BRIDGE CROSS SECTION



NEW BRIDGE USING DETOUR



Pros:

- Accommodates active transportation facilities with standard shoulders and raised trail connection on bridge
- New bridge will be designed for 75-year design life
- Less impact to wildlife, wildlife habitat and vegetation communities due to smaller area of impact compared to options with TMB or new alignment

Con:

- Temporary impact to existing traffic, EMS and trail users for one construction season
- Higher cost for new piers and bridge abutments compared to superstructure replacement
- Potential to impact SAR species due to in-water work and embankment alterations

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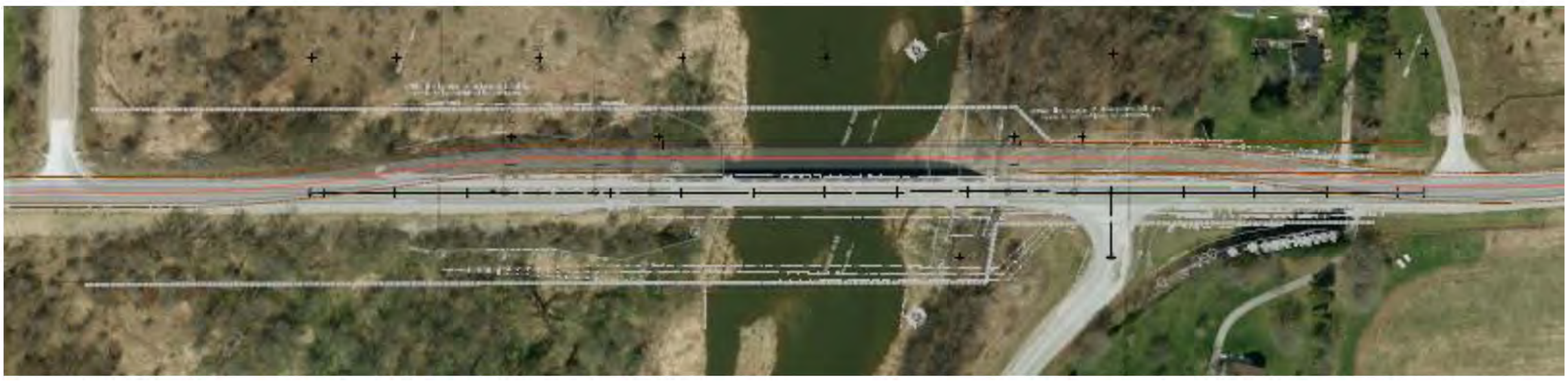
Thorndale Bridge Improvements

Municipal Class Environmental Assessment

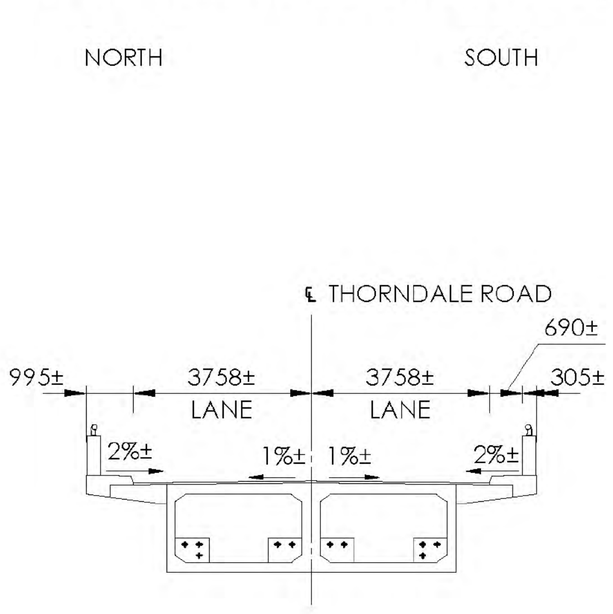
Alternative Solutions

6 New Bridge and Temporary Modular Bridge (TMB)

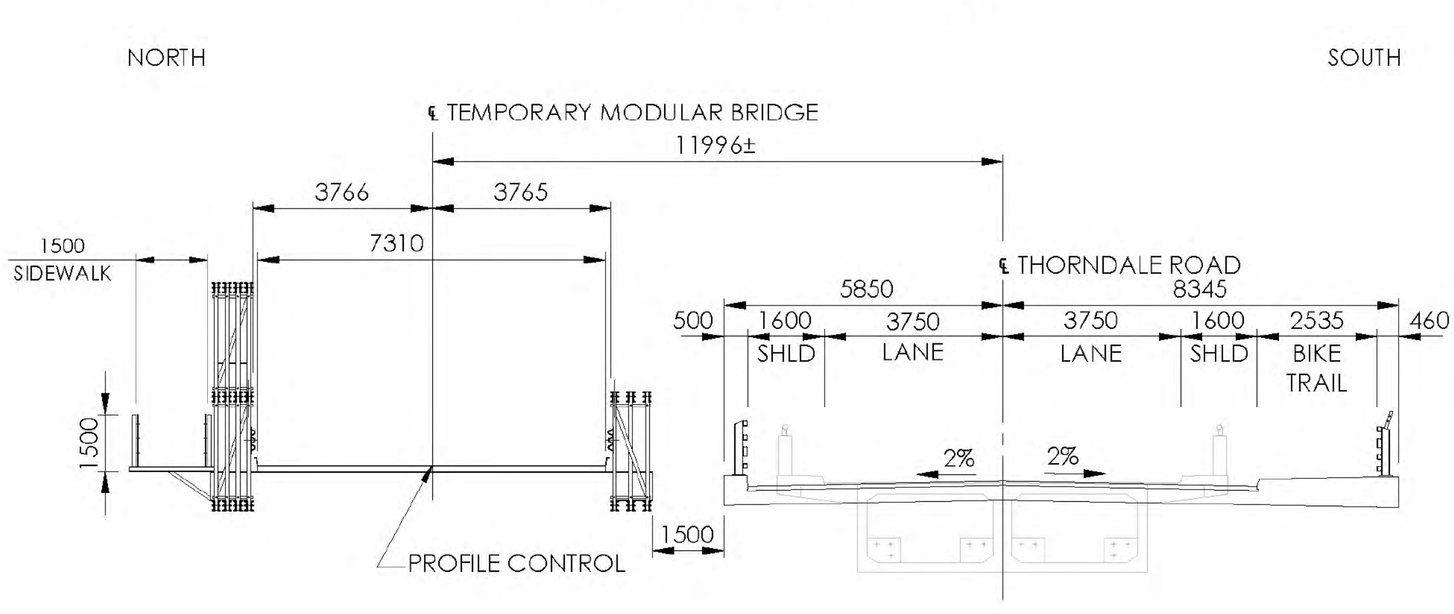
Replace the whole bridge on the existing alignment. Traffic rerouted over TMB on new alignment next to existing bridge.



EXISTING BRIDGE CROSS SECTION



NEW BRIDGE USING TMB



Pros:

- Accommodate active transportation facilities with standard shoulders and raised trail connection on bridge
- New bridge will be designed for 75-year design life
- Minimal impact to existing traffic, EMS and trail users due to TMB

Con:

- High cost for temporary modular bridge
- Higher impact to wildlife, wildlife habitat and vegetation due to TMB and in-water work
- Potential to impact SAR species due to in-water work and embankment alterations
- Impact UTRCA property due to TMB

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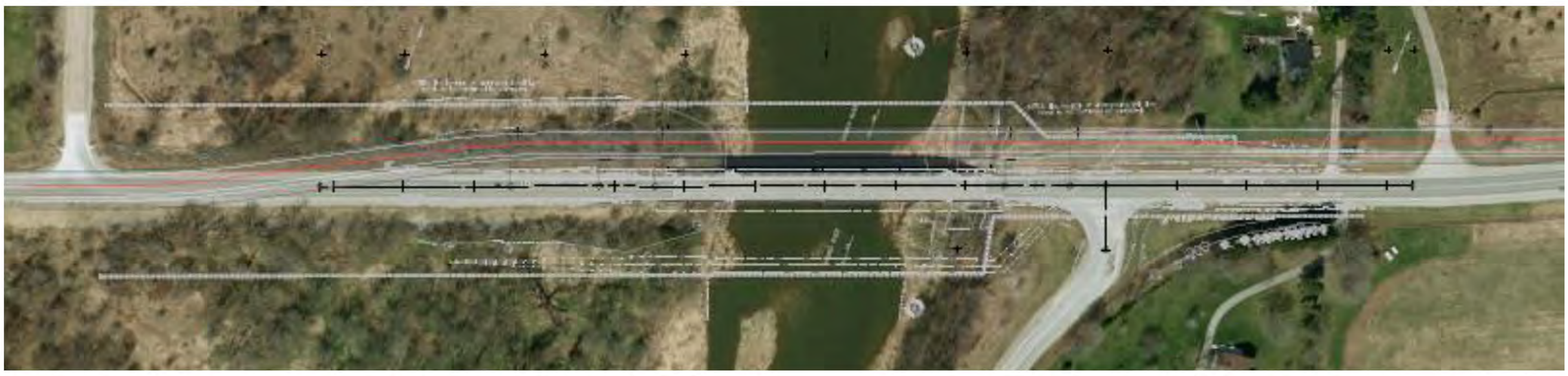
Thorndale Bridge Improvements

Municipal Class Environmental Assessment

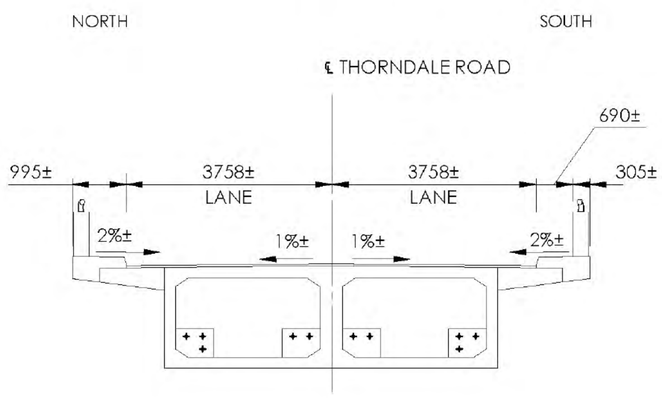
Alternative Solutions

7 New Bridge on New Alignment

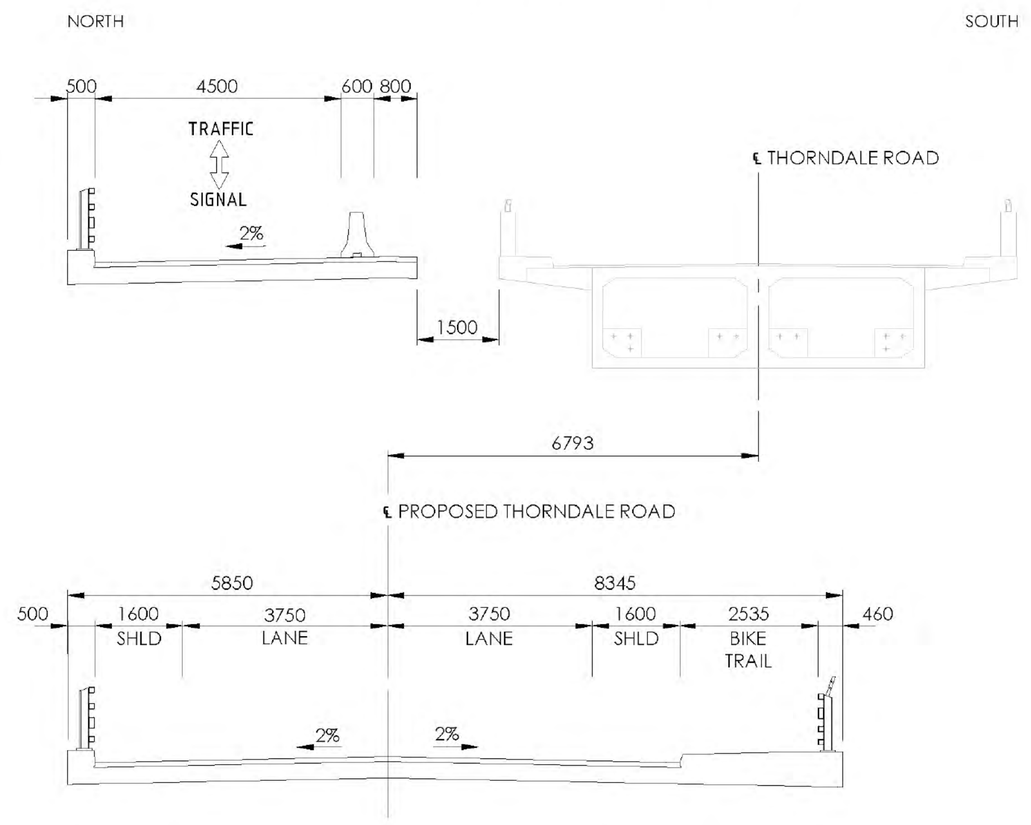
Replace the whole bridge on a new alignment adjacent to the existing bridge. Traffic maintained on existing bridge during construction.



EXISTING BRIDGE CROSS SECTION



NEW BRIDGE ON NEW ALIGNMENT



Pros:

- Accommodates active transportation facilities with standard shoulders and raised trail connection on bridge
- New bridge will be designed for 75-year design life
- Potential to mitigate impacts to existing traffic, EMS and trail users with off-line construction on new alignment

Con:

- High impact to wildlife, wildlife habitat and vegetation due new area of impact with new alignment
- Highest property impact due to permanent new alignment
- Highest overall cost due to new alignment, new piers, bridge abutments and bridge superstructure

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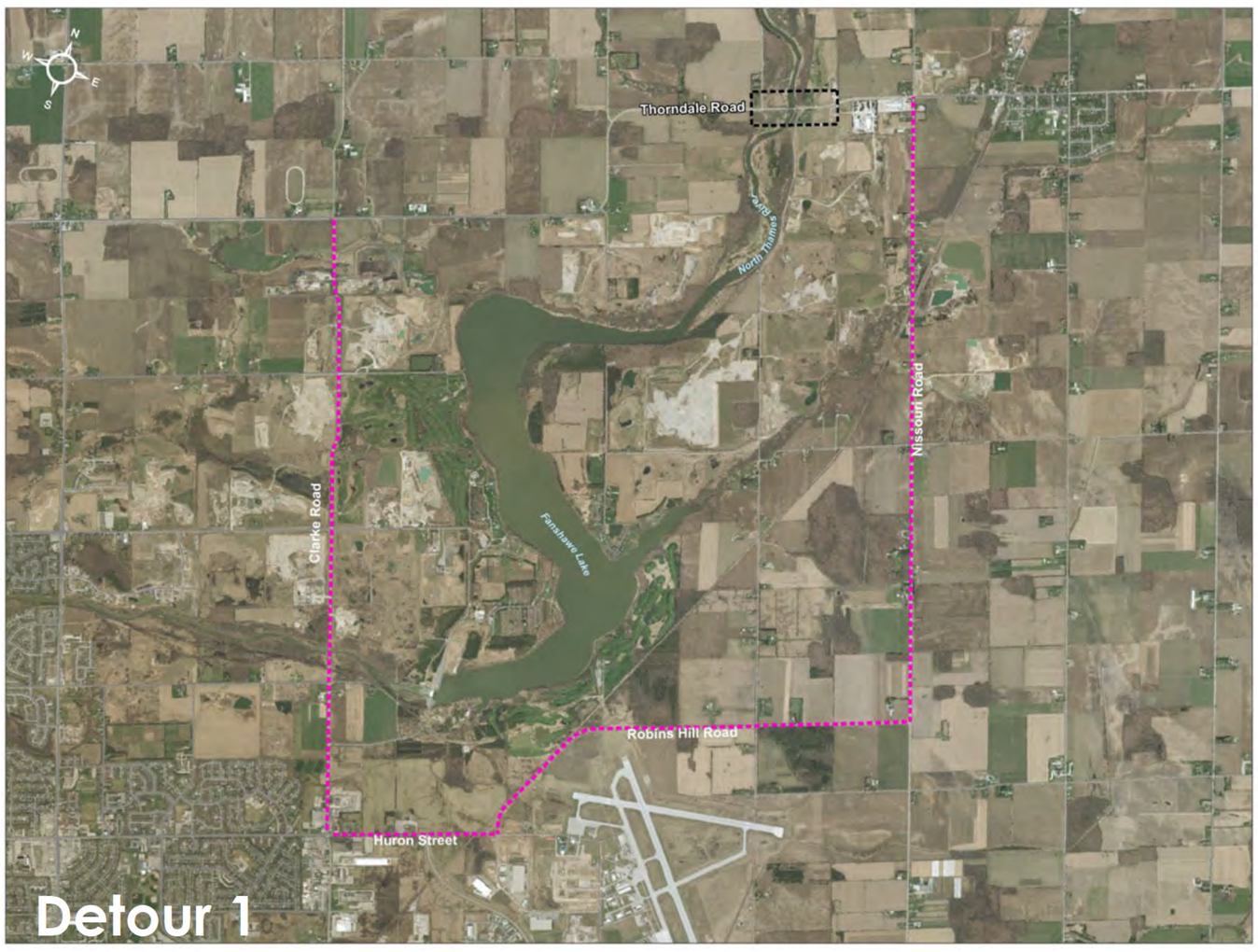
Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Proposed Detour Routes

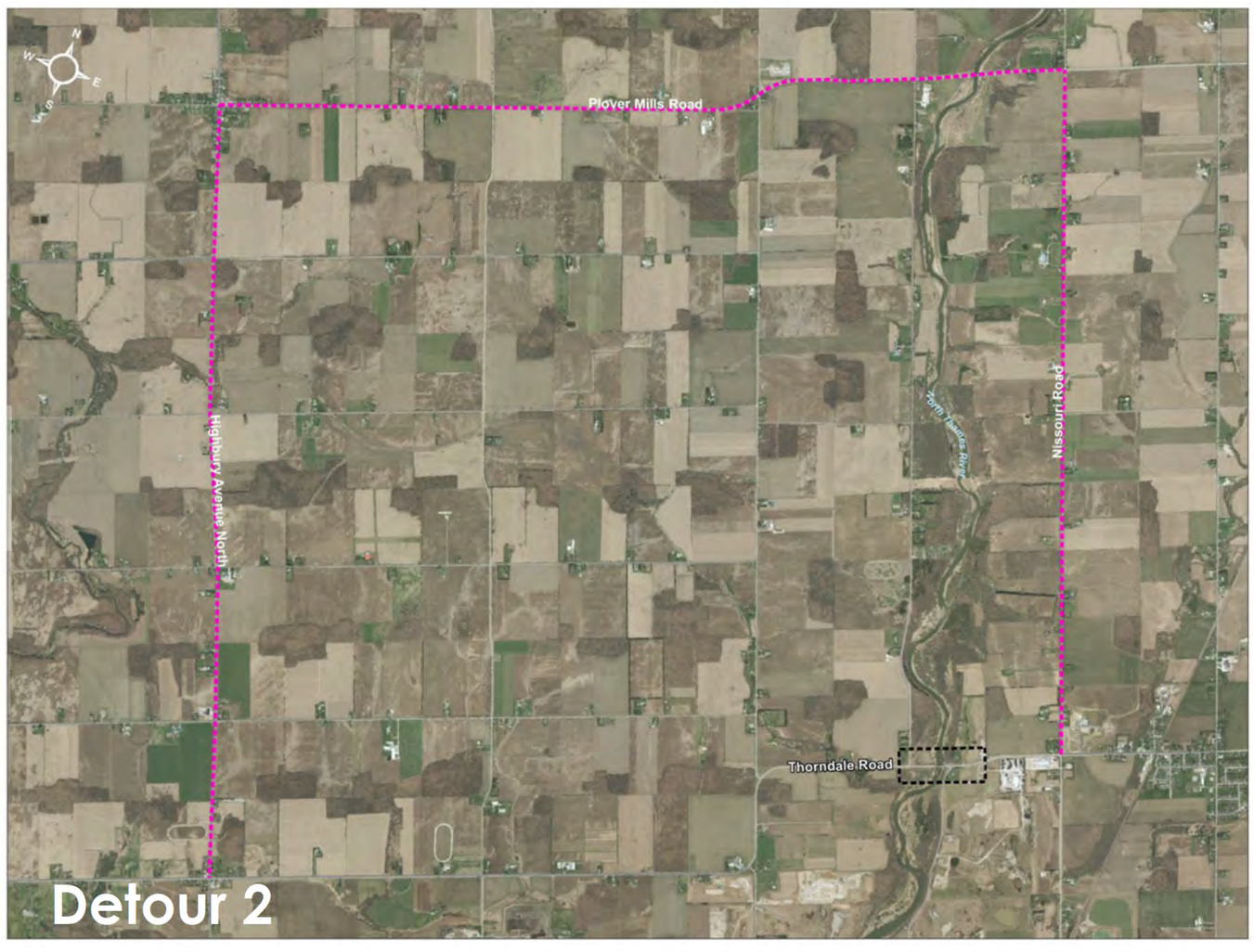
Proposed detour routes were developed to support Alternative Solution 5 (New Bridge and Detour), based on the following criteria:

- Use of County/ City of London roads
- Road must be paved (not gravel)
- Road must be able to accommodate truck/ agricultural traffic



Proposed Detour 1

Approximately 16.8 km



Proposed Detour 2

Approximately 20.8 km

DETOUR

CLOSURE

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Evaluation Criteria

Alternative Designs will be developed to implement the recommended solutions, and will be evaluated against the following criteria:

Socio-Economic Environment

- Property impacts
- Existing/future land uses
- Industrial uses
- Recreational uses
- Business and Agricultural uses
- Property access
- Noise levels
- Accommodation of pedestrians and cyclists
- Air quality

Natural Environment

- Wildlife and wildlife habitat
- Fish and fish habitat
- Vegetation
- Species at Risk
- Drinking water source protection

Cultural Environment

- Built cultural heritage resources
- Archaeological resources
- Cultural heritage landscapes

Transportation

- Traffic operations
- Fire and emergency medical services
- Property accessibility
- Active transportation
- Detours (during construction)

Engineering Considerations

- Structural requirements (Thorndale Bridge)
- Construction staging
- Municipal services/utilities
- Hydraulic capacity/climate change
- Construction costs

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Assessment of Alternative Solutions

Alternative	Evaluation Summary	Recommendation
Alternative 1 - Do Nothing	Does not address problems and opportunities identified in the study area.	Not recommended for further consideration
Alternative 2 – Rehabilitate the Existing Bridge	Meets requirements for a two lane cross section. Does address County of Middlesex active transportation objectives (buffered paved shoulder) but does not improve connectivity of the Fanshawe Lake Loop trail. Does not address long-term structural needs at the bridge.	Not recommended for further consideration.
Alternative 3 – Replace Superstructure and Detour	Two lane cross section maintained, and ability to accommodate additional active transportation if superstructure widening occurs with standard shoulders. Temporary impacts to existing traffic and trail users due to detour. Remaining service life on existing piers will be shorter than remainder of new structure. Pier strengthening required.	Not recommended for further consideration.
Alternative 4 – Replace Superstructure and Temporary Modular Bridge (TMB)	Same as Alternative 3, although a TMB accommodates traffic adjacent to the existing structure. Minimal impacts to traffic during construction due to TMB. Remaining service life on existing piers will be shorter than remainder of new structure. Pier strengthening required. Some additional natural heritage impacts due to the TMB.	Not recommended for further consideration.
Alternative 5 – New Bridge and Detour	Two lane cross section maintained, and ability to accommodate active transportation. High potential for temporary impacts to existing traffic and trail users due to detour. High potential for Emergency Medical Services(EMS) delays due to detour route. Designed for a 75-year lifespan.	Carry forward for further consideration ✓
Alternative 6 – New Bridge and Temporary Modular Bridge (TMB)	Same as Alternative 5, although detours are not needed due to TMB. Minimal impact to traffic, EMS, and trail users during construction due to TMB. Designed for a 75-year lifespan. Some additional natural heritage/ property impacts due to the TMB footprint.	Carry forward for further consideration ✓
Alternative 7 – New Bridge and New Alignment	Two lane cross section with new structure and ability to accommodate active transportation. Minimal impact to traffic during construction due to construction offline. Designed for a 75-year lifespan. Higher natural heritage/property impacts due to new alignment.	Carry forward for further consideration ✓

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Next Steps

We Are Here

Phase 1

Phase 2

Phase 3

Phase 4

Phase 5

Review, address and incorporate comments received on the existing conditions, the recommended alternative solutions and preliminary design alternatives.

Confirm the **Preferred Solution.**

Develop **Design Alternatives** to implement the preferred solution and assess based on the draft criteria presented on the previous display.

Host **PIC No. 2** to get input on design alternatives and preliminary recommendations (tentatively early 2020).

Prepare an **Environmental Study Report** (ESR) to document the Class EA process. The ESR will be available to review for a minimum of 30 days.

Thank you for attending Public Information Centre No. 1.
Please provide comments by Wednesday October 16, 2019.

Comment sheets are available to fill in this evening or you may provide your comments directly to:

Chris Traini, P.Eng.
County Engineer
County of Middlesex
ctraini@middlesex.ca
519-434-7321 ext. 2347

Isaac Bartlett, P.Eng. ENV. SP.
Project Manager
Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Please feel free to contact us with any questions or comments.

Stantec

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

See attached

☑ **Response Requested**

☐ **Response Not Required**

Please leave your completed comment sheet in the drop box provided or submit (**by October 16, 2019**) to:
Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
600-171 Queens Ave
London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

Name: [redacted]

Mailing [redacted]

(includ [redacted]

Tel: [redacted]

Thank you for coming to Thorndale to show the bridge improvement options proposals. The display was well prepared and presented in a logical order. The people there were open to questions and helped to clarify uncertainties and provided additional information.

It seems to me that doing costly temporary improvements now and then doing a complete bridge replacement in a few years is a waste of taxpayers' money.

I think the most cost effective proposal is, (I think), option #5. The new bridge on same line of travel with no temporary bridge. I forgot to ask how long that project would take to complete.

The gentleman I spoke with said he would send me the presentation via email if requested. If that is possible, I would like to have a closer look at the other factors such as environmental impact.

COMMENT FORM

Thorndale Bridge Improvements Municipal Class EA

Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

As a local farmer transporting machinery across the bridge I have concerns about a new bridges ability to accomadate wide loads. Paved shoulders to accomadate cyclists would be good and also accomade wide loads. A pedestrian side walk would also be nessasary and could also provide an escape root for cyclists on the bridge while wide machinery is crossing bridge. Alternative #5 is probably the best option as long as it impacts only one season. Keeping the public informed of a construction sceduls would be important as I could plan my cropping to minimize my own disruption to my farming.

☒ **Response Requested**

☐ **Response Not Required**

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London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

[redacted]

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

Your comments will help us to understand what is important to people in the study area. Please provide your comments and use the back of this sheet if you need more space.

Comments: GREAT IDEA TO INVITE PUBLIC COMMENT. MUCH APPRECIATED.

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA

[redacted]

Bridge definitely needs to be widened. Very unsafe for pedestrians, cyclists, snowmobiles & ATV's as well as drivers who have to work their away around.

Definetly need paved shoulders. many many cyclists on this road.

Please put up proper detour signs this time. Bridge was closed for many months in 2002 + 2017 again In 2017 proper signage was not put up and many vehicles got to Prospect Hill Road coming from the west and then discovered bridge was closed. This put lots of traffic onto Prospect Hill Road to the point where the road is basically destroyed. Very dangerous Unless you know the →

☐ **Response Requested**

☑ **Response Not Required**

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Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

Name: [redacted]

Mailing Addre

(include postal code)

Tel: Fax: Email: [redacted]

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

Your comments will help us to understand what is important to people in the study area. Please provide your comments and use the back of this sheet if you need more space.

Comments:

Sections that are "Washboard"ed If you hit it going the speed limit at the right angle because you don't know its there it will almost ~~through~~ throw you into the ditch

The worst I saw without having the proper signage up was when a transport ended up at the closed bridge - 3:00 am. - pitch black - rainy and he had no choice but to back up the hill & through the curves to get to prospect hill - I'm not sure how he did that, but he did.

I had a big chuckle this year when a sign went up in Balleymote to say Missouri Road bridge was closed yet they couldn't put one up there when that actual road - Medway - was closed.

Why can we not be like other places that have a ~~[illegible]~~ light system to allow traffic to still go through - at least for part of the time that it is closed.

I am not as concerned about fire ambulance etc as we will be covered by Arva / London but I think people in Thorndale should be very concerned about ambulance times.

Thanks!

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

My feeling is to replace the bridge in its entirety @ the same time to eliminate further expense down the road. Would it be possible to use stop lights rather than a detour. To shorten EMS, Fire the response time? If not detour #1 is a shorter route.

Just wondering if a bike bridge downstream from the bridge would be feasible. This would keep the trail off the bridge and increase the safety of the cyclists - they ride out of the trail quickly & often ride 2-3 abreast + a car coming down hill from West or East @ 80km increases the risk of collision

The deck of the bridge needs to be wide enough to accomodate a few more pieces of new + bigger farm equip't.

☐ **Response Requested**

☑ **Response Not Required**

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Project Manager
Stantec Consulting Ltd.
600-171 Queens Ave
London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

[redacted]

Mailing Address:

(include postal code)

Tel: Fax: Email:

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

Any alternative that does not include detour is preferred. Analysis seems not to take into account significant disruption to the community that a detour involves. The last rehabilitation that involved a ~~detour~~ long term detour significantly affected the community.

☐ **Response Requested**

☐ **Response Not Required**

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Project Manager
Stantec Consulting Ltd.
600-171 Queens Ave
London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

Name: [redacted]

Mailing Address [redacted]

(include postal [redacted]

Tel: Fax: Email:

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

Yes I would like to see an improvement on the bridge. Mostly for cyclist and pedestrians

#6 is first choice
#7 2nd choice

☐ **Response Requested**

☑ **Response Not Required**

Please leave your completed comment sheet in the drop box provided or submit (**by October 16, 2019**) to:
Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
600-171 Queens Ave
London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

Name: [redacted]

Mailing Address: [redacted]

(include postal code)

Tel [redacted] Email: [redacted]

COMMENT FORM
Thorndale Bridge Improvements Municipal Class EA
Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

- Bicycles + Pedestrians walking along bridge area is very dangerous. Better signs of area for them coming out of trees west of bridge.
- if a detour is decided as the best option the winter maintenance along Robin Hill Road needs to be improved. Many winter days I have travelled that road morning & night and the snow plows have not been on them.
- a temporay bridge beside the existing bridge would be less stress and more convenient by far for the community

☐ **Response Requested**

☒ **Response Not Required**

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Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
600-171 Queens Ave
London, ON N6A 5J7
Tel. (519) 675-6643 Email: isaac.bartlett@stantec.com

Name and Address (optional) PLEASE PRINT

Name:

Mailing Address:

(include postal code)

Tel: Fax: Email:

COMMENT FORM

Thorndale Bridge Improvements Municipal Class EA

Public Information Centre 1, Thorndale Community Centre – Wednesday, September 25, 2019

Your comments will help us to understand what is important to people in the study area. Please provide your comments and use the back of this sheet if you need more space.

Comments: Best option.
alternative #5. Replace with new Bridge. although inconvenient for one season. appears to be the most cost efficient & least Environmental Impact.
The new Bridge will be Complete Piers - Deck and all. Therefore only Minor Maintenance for the next 75 years

alt - Re alignment.
A good option - use existing Bridge while new being Constructed.
Pending cost + Design - old Bridge could be refurbished to Maintain Bike - Pedestrian Traffic for Trail Connection.

New bridge Should be the Bridge for the future (75 years) - Lots wide. - Agriculture Equip wide Bike path walkway

APPENDIX A.4

Public Information Centre 2

NOTICE OF PUBLIC INFORMATION CENTRE #2
Thorndale Bridge Improvements, Municipal Class Environmental Assessment

Middlesex County is undertaking a Municipal Class Environmental Assessment (EA) study for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road).

The existing bridge is approximately 67 years old and has been identified for replacement within the next 10 years. Following the first Public Information Centre (PIC), three alternative solutions to replace the existing bridge with a new structure were carried forward for further evaluation. To effectively assess the temporary and permanent impacts associated with each alternative solution, the evaluation considered a range of quantitative and qualitative factors and criteria. Based on the evaluation, a recommended alternative has been selected for review and input from members of the public and agencies.

The study is being undertaken in accordance with the requirements for Schedule 'C' projects within the Municipal Class EA document (October 2000, as amended in 2007, 2011 & 2015), under the *Ontario Environmental Assessment Act*.

A key component of the study is consultation with interested stakeholders (public and regulatory agencies) through PICs. The second, and final, PIC for this study will be held on:

Date:	Thursday, February 13, 2020
Time:	Drop-in between 5:30 – 7:30 p.m.
Location:	Thorndale Community Centre 265 Queen Street, Thorndale ON

The purpose of the second PIC is to present the alternative design concepts for replacement of the Thorndale Bridge, including the evaluation and potential impacts and proposed mitigation for each alternative. The recommended alternative design will be presented, and public input will be obtained on the design. Anyone with an interest in the study is invited to attend and participate.

If you are unable to attend the PIC and would like to provide comments, please forward them by **Thursday, March 12, 2020**, to either of the following Project Team members:

Chris Traini, P.Eng.
Project Engineer
County of Middlesex
ctraini@middlesex.ca
519-434-7321 ext. 2264

Isaac Bartlett, P.Eng.
Project Manager
Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Personal information collected on this subject is collected under the authority of the *Environmental Assessment Act* and the *Municipal Freedom of Information and Protection of Privacy Act* for transparency and consultation purposes. With the exception of personal information, comments and information received will be maintained on file for use during the study and may be included in project documentation.

This Notice was issued on January 28, 2020.

Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Welcome

Thank you for attending the Public Information Centre (PIC) for the **Thorndale Bridge Improvements Municipal Class Environmental Assessment**

THE PURPOSE OF TONIGHT'S PIC:

The purpose of the PIC is to present the alternative design concepts for replacement of the Thorndale Bridge, including the **evaluation,** potential **impacts** and proposed **mitigation** for the **recommended alternative design**.

WE NEED YOUR INPUT ON:

- The **evaluation** of alternative designs solutions
- The **recommended alternative design** solutions
- Any **additional information** you would like us to consider and/or incorporate into the study
- Comment sheets are available, and we encourage you to fill one out this evening or submit it to the project team by **Thursday, March 12, 2020**.

Study Area

The study area includes the Thorndale Road bridge, located on Thorndale Road (County Road 28), east of Valleyview Road, west of Rebecca Road and approximately120 m north and south of the bridge.

Thorndale Road is an east-west arterial road that provides connectivity between the communities of Thorndale, Ballymonte and Arva.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

What Is The EA Process?

The Municipal Class EA is an approved process under the Ontario Environmental Assessment Act which municipalities follow for the planning and design of municipal infrastructure projects. The process:

- Identifies needs, problems and opportunities
- Considers a range of reasonable solutions
- Requires public, agency, and Indigenous community consultation
- Documents the decision-making process in a clear and transparent manner.

This study is being planned as a Schedule 'C' project, which involves the completion of Phases 1 through 4 of the planning process.

Phase 1
Problems or Opportunities
← Identify and describe the problems and/or opportunities.

▼

Phase 2
Alternative Solutions
← Prepare environmental inventory, identify/assess alternative solutions and seek public and agency comments.

PIC 1
Sep 25, 2019

▼

Phase 3
Alternative Designs for Preferred Solution
← Develop and assess alternative designs, identify potential environmental impacts and proposed mitigation measures.

We are here

PIC 2
Feb 13, 2020

▼

Phase 4
Environmental Study Report
← Prepare and file an Environmental Study Report (ESR) for a minimum of 30 calendar days period to allow for public and agency review.

▼

Phase 5
Implementation
← Construct project as described in the ESR (subject to future municipal capital budgets).

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Problem/Opportunity Statement

Based on the review of the existing conditions, municipal priorities, planning and policy documents, and future development, the following summarizes the problems and opportunities within the study area:

Active Transportation

There is a need to improve active transportation facilities within the study area (buffered paved shoulder identified in Cycling Master Plan) and provide connections to the Fanshawe Lake Trail System.

Bridge Condition

The existing bridge is 67 years old and has been identified for replacement within the next 10 years.

This study will consider alternatives for replacement of the existing structure. The bridge will provide sufficient road capacity, while safely and efficiently accommodating active transportation.

Existing Thorndale Bridge facing north

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Alternative Solutions

Improvements to the Thorndale Bridge are required to address the existing bridge condition and to address the opportunity to improve active transportation facilities and connections to the Fanshawe Lake Trail System. The following alternative solutions were presented at PIC #1

1 **Do Nothing**
No proposed changes to the bridge. This alternative is included to provide a base to which other alternatives can be compared.

2 **Rehabilitation of Existing Bridge**
Maintain existing bridge and repair sections as required

3 **Replace Superstructure and Detour**
Strengthen existing piers and rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted around bridge construction on detour.

4 **Replace Superstructure and Temporary Modular Bridge (TMB)**
Strengthen existing piers, rebuild the walls and a wider bridge deck to include standard shoulders or sidewalk. Traffic rerouted over TMB on new alignment next to existing bridge

Carried forward for a more detailed review and analysis

5 **New Bridge and Temporary Detour**
Replace the whole bridge on the existing alignment. Traffic rerouted around bridge construction on detour.

6 **New Bridge and Temporary Modular Bridge (TMB)**
Replace the whole bridge on the existing alignment. Traffic rerouted over TMB on new alignment next to existing bridge.

7 **New Bridge on a New Alignment**
Replace the whole bridge on a new alignment adjacent to the existing bridge. Traffic maintained on existing bridge during construction.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Alternative Solutions

The following alternative solutions presented at PIC #1 were carried forward for a more detailed review and assessment

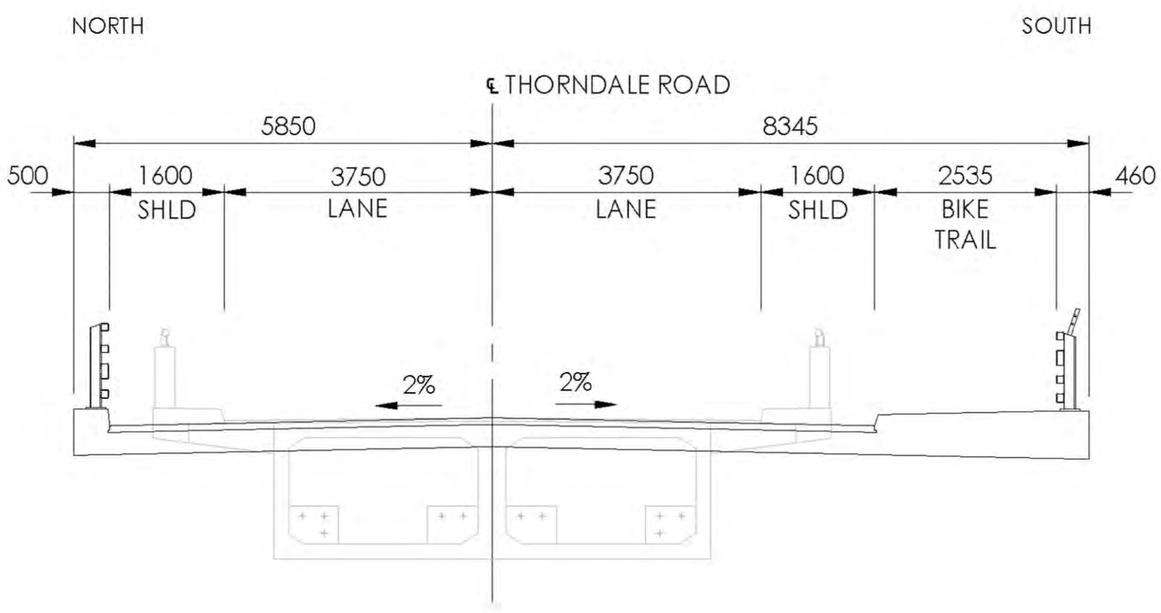
5

New Bridge and Temporary Detour

Two lane cross section maintained, with the ability to accommodate active transportation.

Detour route implemented during construction.

Designed for a 75-year lifespan.



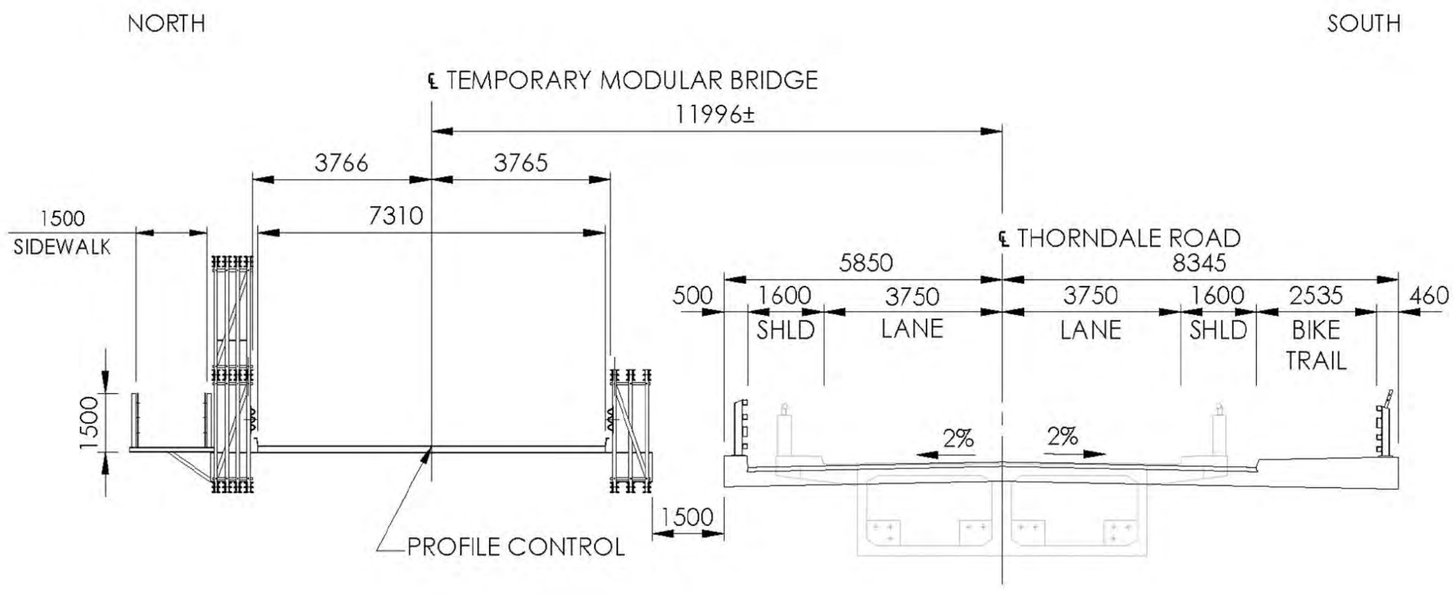
6

New Bridge and Temporary Modular Bridge

Two lane cross section maintained, with the ability to accommodate active transpiration.

Temporary Modular Bridge installed to maintain traffic flow during construction.

Designed for a 75-year lifespan.



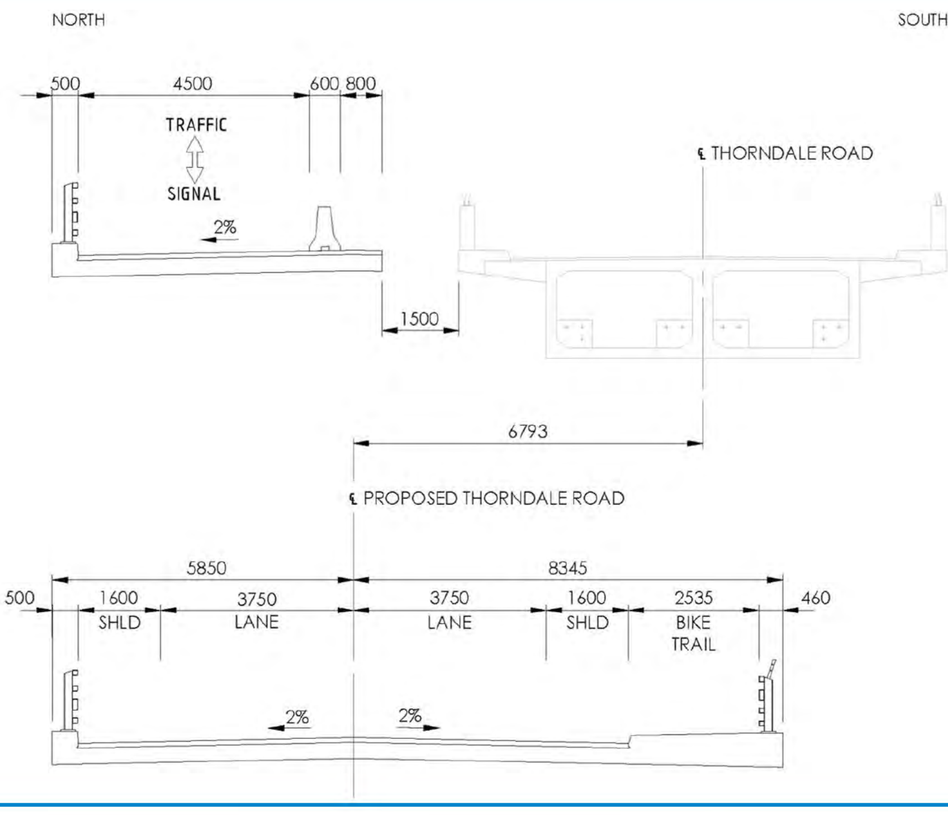
7

New Bridge on a New Alignment

Two lane cross section with new structure, with the ability to accommodate active transportation.

Constructed offline. Existing bridge to be used to maintain traffic flow during construction.

Designed for a 75-year lifespan.



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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Evaluation Process

The Project Team has evaluated the potential temporary and permanent impacts of each **Alternative Solution**. A summary of the evaluation is presented on the following panels.

The **evaluation criteria** used to assess the Alternative Solutions considered both qualitative and quantitative measures. Where possible, quantitative measures were used to compare the advantages and disadvantages of each Alternative Solution in numeric terms. Qualitative methods were used to describe the advantages and disadvantages for each criteria that are not easily measured or quantified.

In addition, criteria used to evaluate the alternative solutions were carried forward into the **detailed evaluation** of Alternative Solutions when significant differences between the alternatives were recognized.

Detailed evaluation will be provided in the Environmental Study Report (ESR) or upon request.

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Evaluation Criteria

Alternative Solutions carried forward from PIC 1 were evaluated against the following factors and criteria to assess the potential temporary and permanent impacts associated with each solution:

Socio-Economic Environment

Temporary Impacts
- Property Impacts

Permanent Impacts
- Property Acquisition and Impacts

Natural Environment

Temporary Impacts
- Aquatic Species and Habitat
- Wildlife and Wildlife Habitat
- Vegetation
- Species at Risk

Permanent Impacts
- Aquatic Species and Habitat
- Wildlife and Wildlife Habitat
- Vegetation
- Species at Risk

Cultural Environment

Temporary Impacts
- N/A

Permanent Impacts
- Archaeological Resources

Transportation

Temporary Impacts
- Delays to Emergency Medical Services, and Active Transportation
- Property Accessibility

Permanent Impacts
- Traffic Operations
- Active Transportation
- Property Accessibility

Engineering Considerations

Temporary Impacts
- Construction Duration
- Structural Complexities
- Constructability
- Municipal Services and Utilities
- Hydraulic Capacity
- Cost

Permanent Impacts
- N/A

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Evaluation of Temporary Impacts

Factors	Evaluation Criteria	Alternative 5 New Bridge and Temporary Detour	Alternative 6 New Bridge and Temporary Modular Bridge (TMB)	Alternative 7 New Bridge on a New Alignment
Transportation Environment				
Delays to Emergency Medical Services (EMS), and Active Transportation	• Impacts to EMS response time, public travel time, and active transportation access (Fanshawe Lake Loop Trail) during construction	○	●	●
Property Accessibility	• Temporary impacts to existing property access during construction	●	◐	○
Socio-economic Environment				
Property Impacts	• Temporary Impacts to property and existing land use	●	○	●
Natural Environment				
Aquatic Species and Aquatic Habitat	• Temporary impacts to aquatic species and habitat • Species at Risk (Silver Shiner, Rayed Bean mussel) • Impacts to in-water sensitive features during construction (i.e., area and instances of in-water work required)	●	○	◐
Wildlife and Wildlife Habitat	• Temporary impacts to wildlife and wildlife habitat (i.e., bat roosts, turtle wintering, snake hibernaculum, seeps)	●	○	◐
Vegetation	• Temporary impacts to vegetation communities, particularly special concern and provincially rare plant species	●	○	◐
Engineering				
Construction Duration	• Potential impact due to the total length of time to construct	●	○	◐
Structural Complexities	• Ability to maximize structural capacity and durability (complexity of the design, including construction, staging, and long-term maintenance)	●	●	●
Constructability	• Potential for difficulties and risks during construction (a more complex construction approach tends to take more time, cost more, and introduces additional construction stages that could impact road users)	●	○	○
Municipal Services and Utilities	• Potential impact to municipal services and utilities within the corridor	●	○	○
Hydraulic Capacity	• Potential impact to hydraulic capacity of the structure opening over Thames River	●	◐	◐
Cost	• Relative magnitude cost of construction, including the bridge removal and replacement, any temporary works, utilities, maintenance and property	●	◐	○
Summary of Temporary Impacts		**Most Preferred**	Least Preferred	Moderately Preferred

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Evaluation of Permanent Impacts

Factors	Evaluation Criteria	Alternative 5 New Bridge and Temporary Detour	Alternative 6 New Bridge and Temporary Modular Bridge (TMB)	Alternative 7 New Bridge on a New Alignment
Transportation Environment				
Traffic Operations	• Ability to accommodate future travel demands	●	●	●
Active Transportation	• Potential to accommodate future active transportation facilities • Ability to improve Fanshawe Loop Trail connectivity	●	●	●
Property Accessibility	• Impacts to future property access	●	●	●
Socio-economic Environment				
Property Acquisition and Impacts	• Property to be permanently acquired and/or impacted	●	◐	○
Natural Environment				
Aquatic Species and Aquatic Habitat	• Permanent impacts to aquatic species and habitat • Area of floodplain disturbance +30 m buffer required for Silver Shiner (Species at Risk) • Species at Risk (Silver Shiner, Rayed Bean mussel) • Impacts to in-water sensitive features during construction (i.e., area and instances of in-water work required)	●	○	◐
Wildlife and Wildlife Habitat	• Impacts to wildlife and wildlife habitat (i.e., bat roosts, turtle wintering, snake hibernaculum, seeps) • Species at Risk (Meadowlark, Bats)	●	◐	○
Vegetation	• Permanent impact to vegetation communities, particularly special concern and provincially rare plant species (Butternut)	●	◐	○
Cultural Environment				
Archaeological Resources	• Potential impacts to lands with archaeological potential	●	○	◐
Summary of Permanent Impacts		**Most Preferred**	Moderately Preferred	Least Preferred

Based on the detailed evaluation, a new bridge using a temporary detour during construction is the recommended solution to be carried forward to the next phase of the study.

Legend

- ● Most Preferred
- ◐ Moderately Preferred
- ○ Least Preferred

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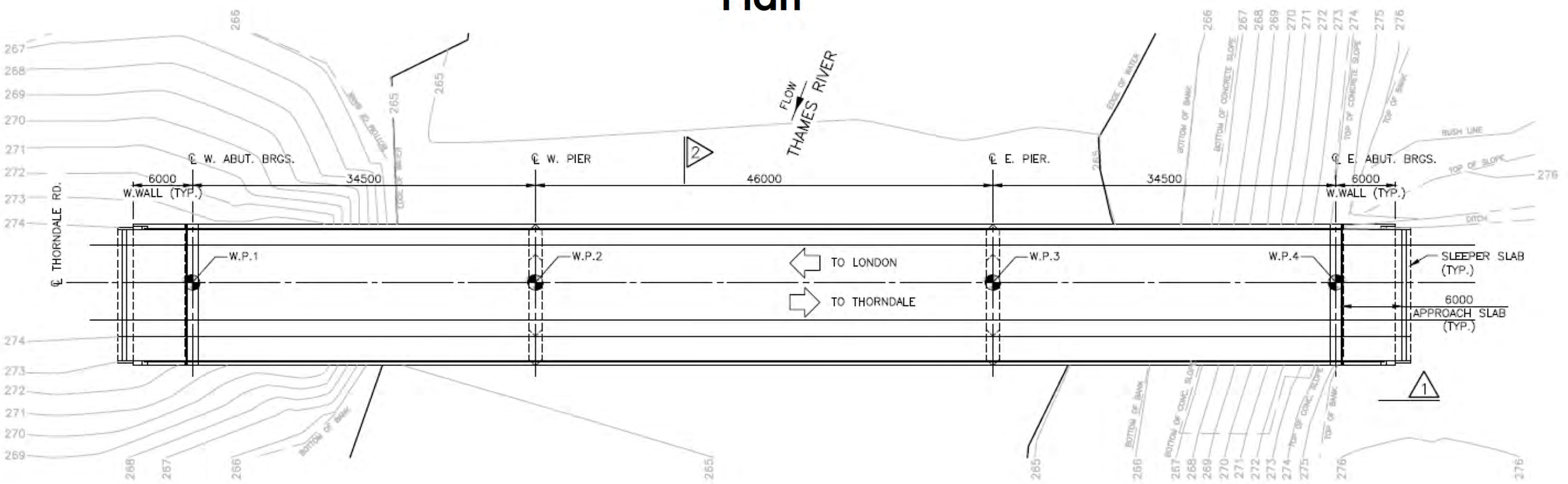
Thorndale Bridge Improvements

Municipal Class Environmental Assessment

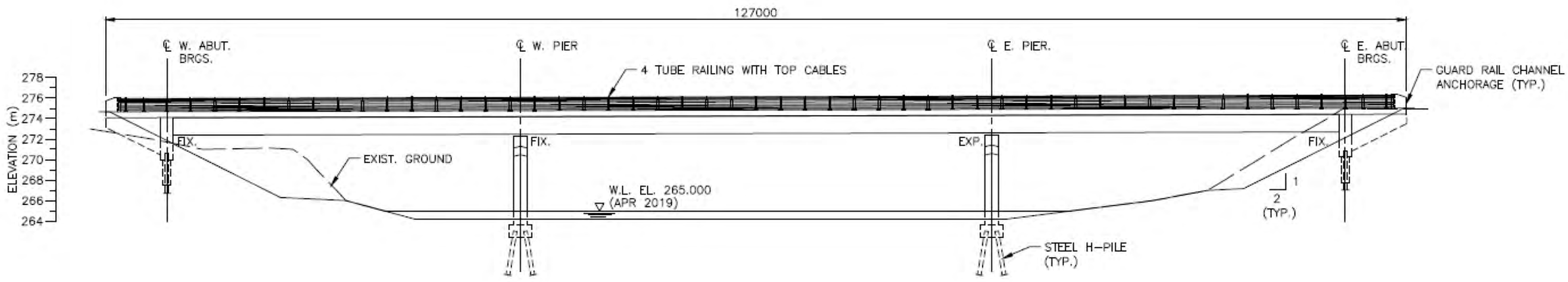
Recommended Solution

General Arrangement Design

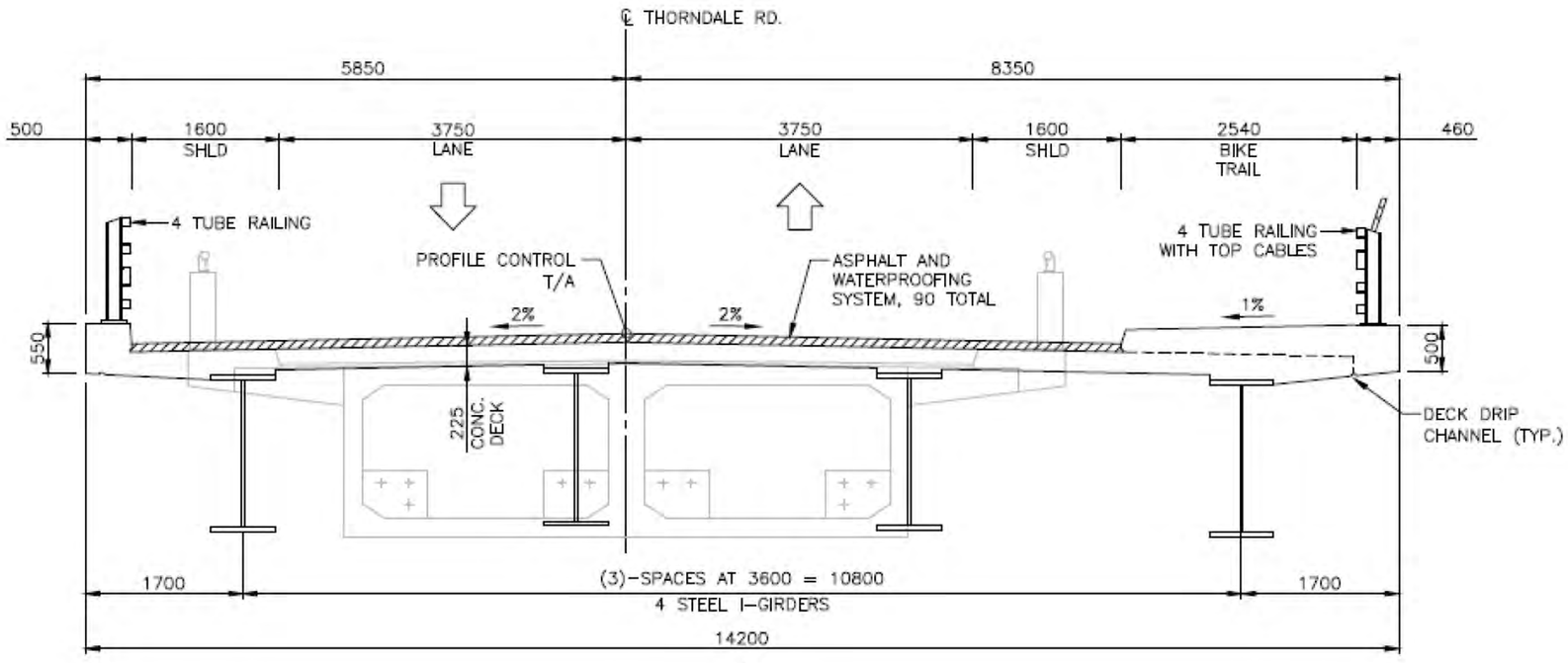
Plan



Profile



Cross Section



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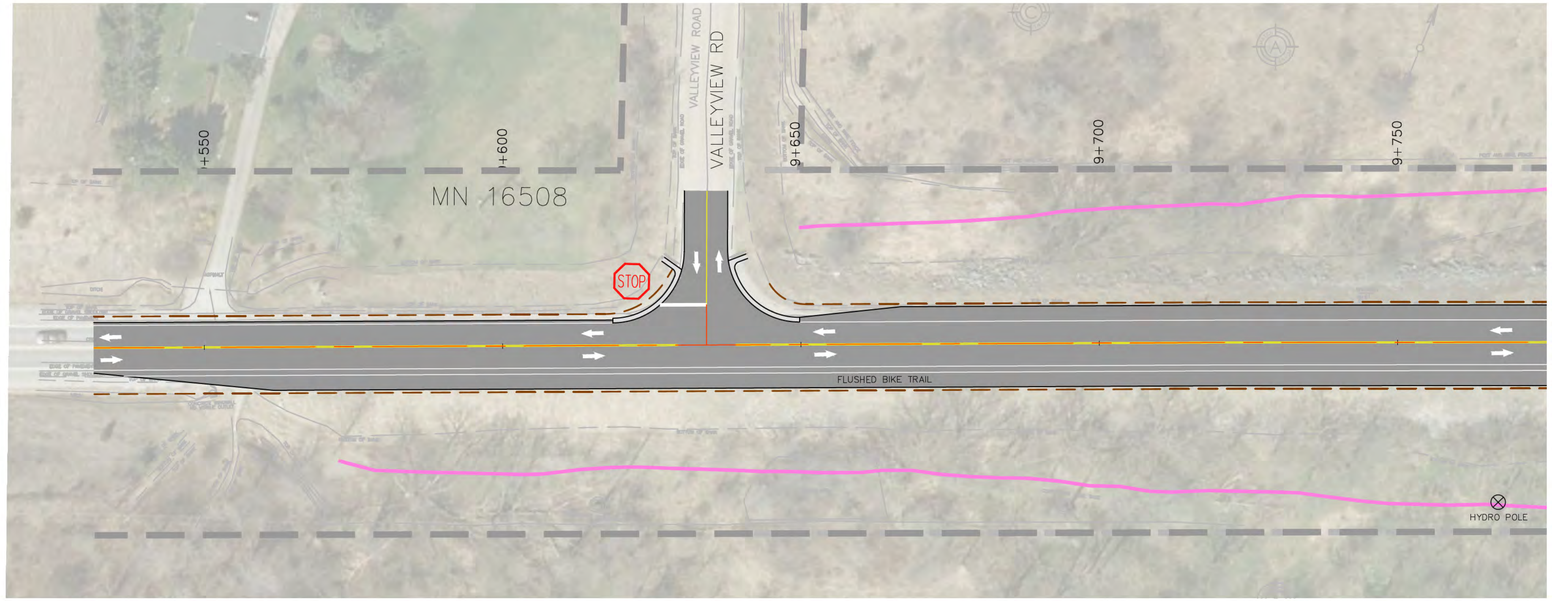
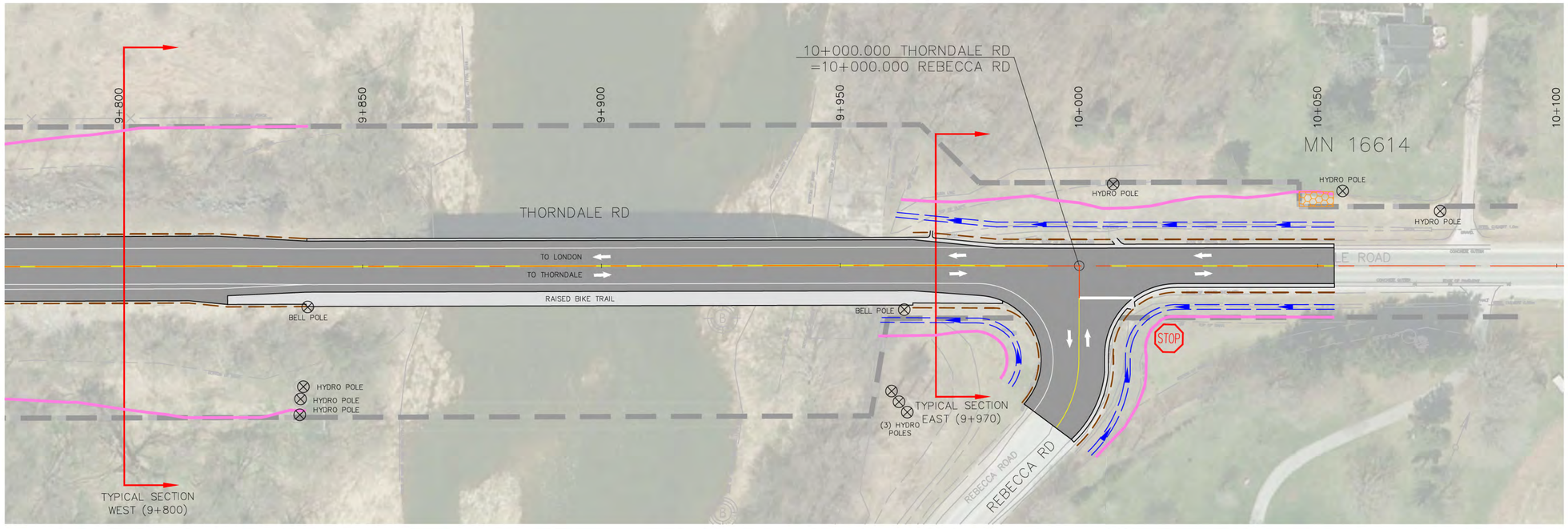
SUPLEMENTARY LEGEND

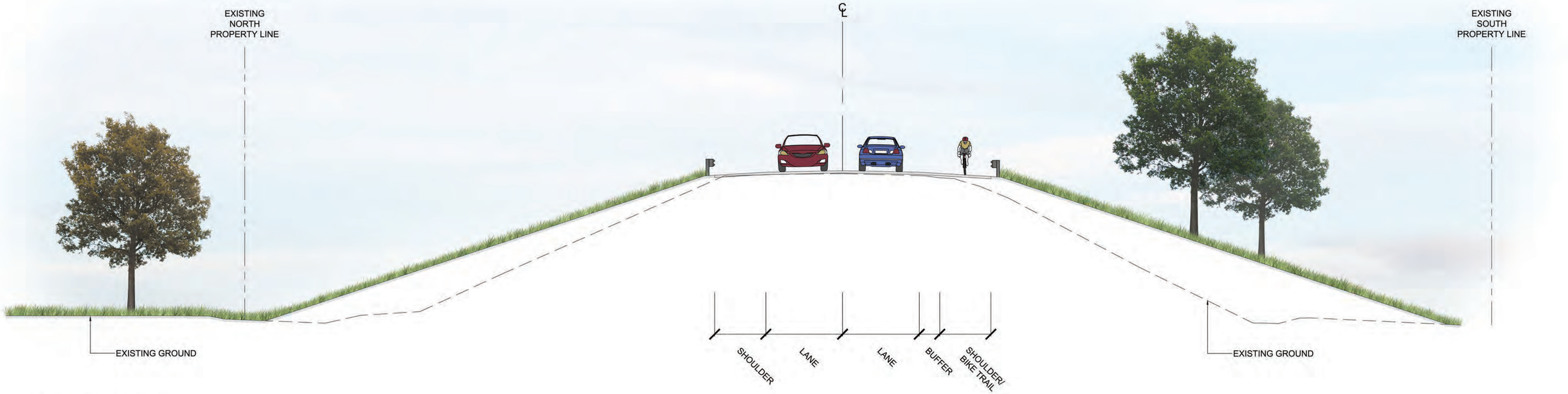
- ANTICIPATED PROPERTY IMPACTS
- ASPHALT ROADWAY
- GRADE LIMIT
- NEW DITCH LINE
- GRANULAR SHOULDER
- EXISTING PROPERTY LINE



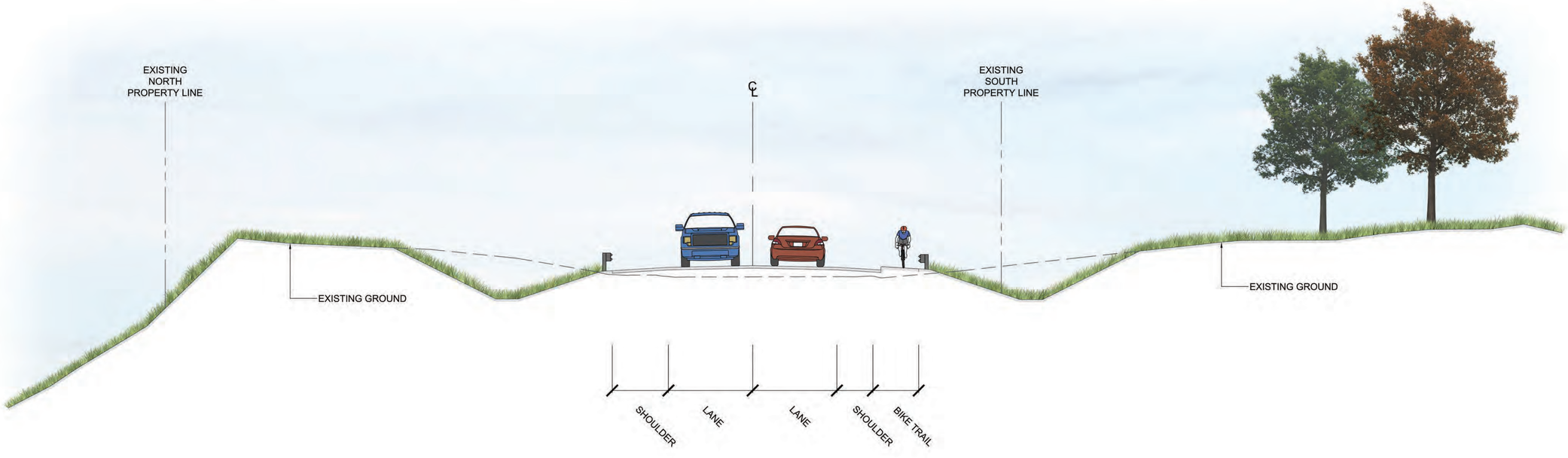
THORNDALE ROAD



THORNDALE ROAD



Typical Section West (9+800)



Typical Section East (9+970)

Thorndale Bridge Cross-Sections

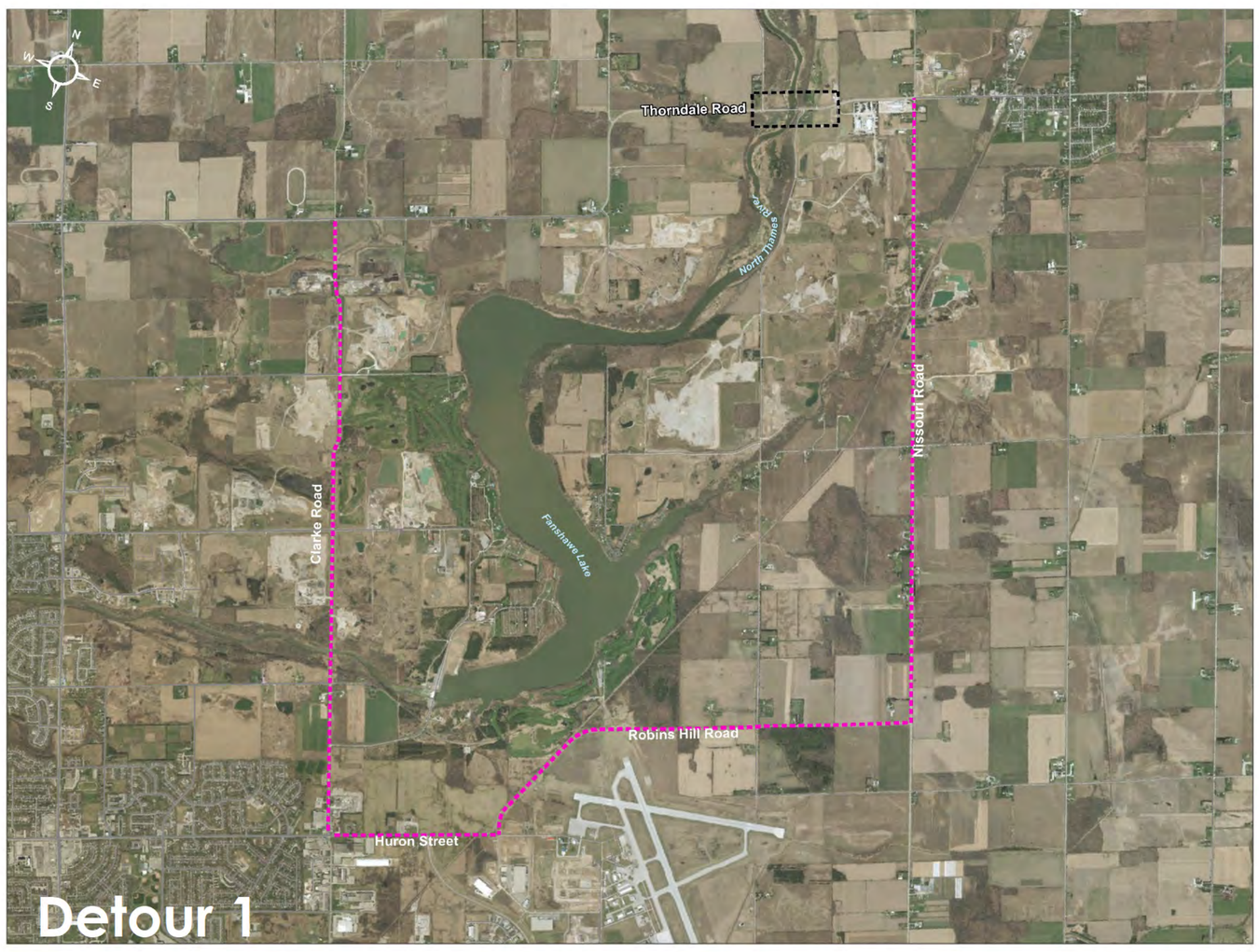
Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Proposed Detour Routes

Proposed detour routes were developed to support a new bridge and temporary detour based on the following criteria:

- Use of County/ City of London roads
- Road must be paved (not gravel)
- Road must be able to accommodate truck/ agricultural traffic

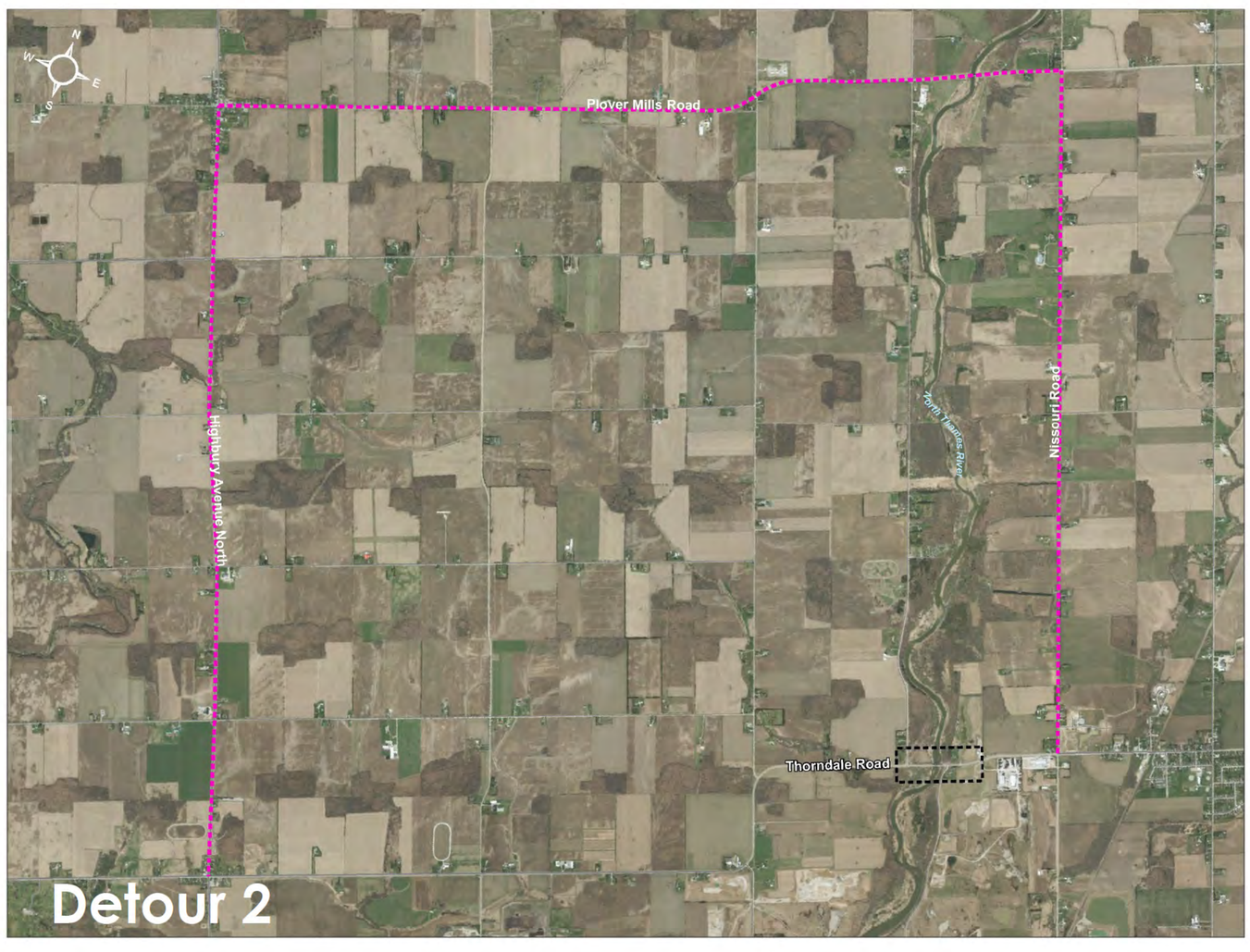


Proposed Detour 1

Approximately 16.8 km

Proposed Detour 2

Approximately 20.8 km



DETOUR

CLOSURE

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Proposed Mitigation Measures

Key highlights include:

Socio-economic

- Traffic delays will be minimized to the extent possible
- Continue to work with impacted property owners during the detail design and construction phases

Natural Environment

- Limit encroachment into natural vegetation through design and construction methods
- Adhere to timing windows for vegetation clearing and in-water work to avoid harm to fish, mussels and wildlife
- Stabilize bank with erosion protection/native plantings in riparian area after construction is complete

Cultural Environment

- A Stage 2 Archaeological Assessment will be completed at the detailed design phase, as required

Transportation

- Advanced notification of construction start and detours will be provided via newspaper notices, online notifications, property owner mail outs and road signs
- EMS, Municipalities, Upper Thames Conservation Authority, and school boards will be notified in advance of detours

Engineering Considerations

- Utilities in conflict with construction will be relocated prior to construction
- Standard sediment and erosion control measures, including for all in-water works and along the shoreline/riparian area (within 30 m of the normal high-water mark)
- Confirm construction staging during detail design

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Thorndale Bridge Improvements

Municipal Class Environmental Assessment

Next Steps

Meet with agencies and other stakeholders as required

Review, address and incorporate comments received on the detailed evaluation and Recommended Design Alternative.

Prepare an **Environmental Study Report** (ESR) to document the study decision making process and recommendations for late spring/ early summer 2020. The ESR will be available to review for a minimum of 30 days.

Review and address any comments received during the ESR review period.

Thank you for attending Public Information Centre # 2.
Please provide comments by Thursday, March 12, 2020.

Comment sheets are available to fill in this evening or you may provide your comments directly to:

Chris Traini, P.Eng.
County Engineer
County of Middlesex
ctraini@middlesex.ca
519-434-7321 ext. 2347

Isaac Bartlett, P.Eng. ENV. SP.
Project Manager
Stantec Consulting Ltd.
isaac.bartlett@stantec.com
519-675-6643

Please feel free to contact us with any questions or comments.

APPENDIX A.5

Agency Correspondence

Contact	Date	Comment	Response/ Status
Agencies			
Notice of Study Commencement			
Paul Stantos Senior Project Manager MTO West Region	04/11/2019	No concerns with Notice of Study. Requested to be taken off mailing list	
Rob Elliot Construction Project Manager Union Gas	04/16/2019	Stated that Union Gas does not have any infrastructure in area of bridge	
Ministry of Environment, Conservation and Parks	04/16/2019	Acknowledge receipt of Notice of Study email	
I Love Thorndale	04/17/2019	Added Notice of Study Commencement to website to help inform community	
Ministry of Tourism, Culture & Sport (MTCS)	05/07/2019	• Interest in archaeological resources, built heritage resources, and cultural heritage resources • Suggested that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value • Included checklist and background material list for bridge work Requested for HIAs and Archaeological Resources Assessment to be sent to MTCS for review	
Upper Thames River Conservation Authority (UTRCA)	5/24/2019	• Acknowledge receipt of Notice of Study and noted they are involved as a regulatory Authority and a property owner General Comments • Would like their technical staff to review and provide comments on any upcoming draft documents and proposed alternatives and public presentation documents Regulatory Consideration: • Noted portions of the project area are within Natural Hazard and Natural Heritage areas regulated by UTRCA. Suggested a Section 28 permit may be required for work within this area and provided a link to the permit application form Digital Mapping Data: • Will provide access free of charge to digital mapping resources from UTRCA upon request • Hydrology: will provide HEC-RAS geometry and flow files free of charge upon request Bridge Options and Design: • bridge hydraulic capacities should be considered at a minimum the 250 yr return period of flow with consideration of ice blockages/ consideration for upstream/ downstream sedimentation and erosion should be considered Geotechnical Consideration: • depending on project specifics a geotechnical assessment may be required Fish Habitat: • Warm water sportfish community. Recommend any proposed works be planned to mitigate impact. No in-water work or activity should occur at this location from Mach 15th to July 15th . Permits may be required from	

Contact	Date	Comment	Response/ Status
		DFO. Detailed fish/mussel and benthic sampling records may be available from UTRCA. Terrestrial Habitat: • suggest a bird survey, UTRCA does not have any bird records for the specified area. • Terrestrial and Aquatic SAR: UTRCA data suggests potential presence of federally and/or provincially protected aquatic (fish/ mussels) species at risk within study area, federally and or provincially protected terrestrial and botanical SAR in the area and a Critical Habitat in the area. Permits may be required under SAR act and ESA. Water Quality, Woodlands and other Natural Heritage Features: • study area lies within a portion of the Plover Mills Corridor Sub watershed. Landowner comments: • Noted landowner information may be obtained from GIS department at UTRCA Background • trail users use Thorndale Bridge to connect the Fanshawe Lake Trail Recommend bike lanes added to bridge Existing and Proposed Recreational Trail System • Discussion in 2016 with County staff about adding bike lanes to the bridge when future replacement was proposed. County advised that widening of the bridge and accommodation of bike lanes would be considered when the replacement of the bridge was done in the next 10 years Construction Access: • UTRCA requires any consultant, county representative, or agent obtain written permission to entre UTRCA owned lands and Fanshawe Conservation Area by way of a short access agreement for reasons of safety, liability, etc. If bridge rehab/replace then at the detail design stage the UTRCA will require the County of Middlesex to enter into a landowner "Temporary Easement (access – construction) Agreement" prior to commencement of any construction works occurring on OTRCA owned lands and Fanshaw CA.	
Ministry of Environment, Conservation and Parks	05/31/219	• Acknowledge receipt of Notice of Study email • Based on project location the MECP suggested consultation with the following Indigenous Communities:: o Aamjiwnaag First Nation (Sarnia, ON) o Bkejwanong Territory (Walpole Island First Nation) o Chippewas of Kettle and Stony Point First Nation (Forest, ON) o Chippewas of the Thames First Nation (Muncey, ON) o Caldwell First Nation (Leamington, ON) o Oneida Nation of the Thames ONYOTA'A:KA (London, ON) o Munsee-Delaware Nation (London, ON) Eelunaapeewi Lahkeewiit (Delaware Nation) • Request to identify early in the process whether this project is occurring within a source water protection vulnerable area and consult with the appropriate authorities (Conservation Authority) along with documenting it in the ESR. • Suggestion to include climate change in the EA. Climate change should be considered in the context of mitigation and the context of adaptation.	Dear Mr. Newton: Thank you for your letter dated May 31, 2019 in response to the Notice of Study Commencement for the Thorndale Bridge Improvement Municipal Class Environmental Assessment (Class EA) on County Road 28 (Thorndale Road). Your interests regarding Indigenous consultation, Source Water Protection, and Climate Change are noted. The comments have been forwarded to the project team for consideration. **Consultation:** Consultation is a major component of the Municipal Class EA process. As such, consultation with Indigenous communities, federal/provincial ministries, external agencies, stakeholders, and the public will be undertaken throughout the study. A Notice of Study Commencement, Notices of Public Information Centres (PICs) and the Notice of Study Completion will also be provided to the study contact list. The ESR will document all stakeholder consultation efforts and will identify all concerns that are raised and how they are addressed by the project team throughout

Contact	Date	Comment	Response/ Status
		• Request to add the Part II Order form details in the Notice of Completion. • Request for a draft copy of the ESR be sent to the MECP for review a minimum of 30 days for MECP's technical reviews to provided comments on the draft ESR. • All future correspondence with respect to this project should be sent to attention: Craig Newton, Regional Environmental Planner/ Regional EA Coordinator at craig.newton@ontario.ca or (519) 873-5014	the study. All comments that are received will have any personal information redacted in accordance with the *Municipal Freedom of Information and Protection of Privacy Act* and will be included in the ESR. Further to your email, the following Indigenous communities are included on the study contact list: o Aamjiwnaag First Nation (Sarnia, ON) o Bkejwanong Territory (Walpole Island First Nation) o Chippewas of Kettle and Stony Point First Nation (Forest, ON) o Chippewas of the Thames First Nation (Muncey, ON) o Caldwell First Nation (Leamington, ON) o Oneida Nation of the Thames ONYOTA'A:KA (London, ON) o Munsee-Delaware Nation (London, ON) o Eelunaapeewi Lahkeewiit (Delaware Nation), (Thamesville, ON) **Source Water Protection:** The study area is located within the *Thames-Sydenham Source Protection Region* (SPR) and is subject to the policies of the *Thames-Sydenham Source Protection Plan (SPP)*, *Volume III – Policies* affecting the Thames-Sydenham SPR except Oxford County. According to the current mapping (Figure 5), UTRCA *Source Protection Assessment Report* mapping, portions of the study area are located within Significant Groundwater Recharge Areas (SGRA) and Highly Vulnerable Aquifers (HVA) with a maximum vulnerability score of six. Policies of the SPP generally apply to activities considered 'significant threats' to drinking water sources, which can only occur within areas with a vulnerability score of eight or higher. It is not anticipated that improvements associated with this Class EA study will be impacted by existing SPP policies. Source Water Protection information will be documented in the ESR. The Upper Thames River Conservation Authority is included on the project mailing list and engaged as part of the Municipal Class EA. **Climate Change**: Climate change will be considered as part of this study in particular, with respect to present and future rainfall values and site drainage. These considerations will be incorporated into the ultimate bridge design. As stated in the *Provincial Engineering Memorandum: Implementation of the Ministry's Climate Change Consideration in the Design of Highway Drainage Infrastructure*, dated October 28, 2016, all highway drainage designs must take into consideration changes to future rainfall parameters. This is done through the use of the MTO Intensity Duration Frequency (IDF) curves application Version 3 to determine present and future rainfall values. Future rainfall values for the year corresponding to the end of the Design Service Life (DSL) of the structure must be used in the design for conveyance, erosion, scour, and stormwater management components. The design for fish passage shall meet the fish passage design drainage standard requirements at the present and future flow conditions.

Contact	Date	Comment	Response/ Status
			Again, thank you for your comments. We will continue to keep the MECP informed of the project as it progresses. If you have any questions regarding the project, please contact the undersigned. Regards,
Catherine Stewart Management Biologist Permissions and Compliance, Species at Risk Banch Ministry of Environment, Conservation and Parks (MECP)	January 15, 2020 Email (response to NH IR sent by Melissa Cameron Oct. 15, 2019)	RE: Thorndale Bridge over North Thames River, Municipality of Thames Centre, Middlesex County and the *Endangered Species Act, 2007* The Ministry of Environment, Conservation and Parks (MECP) understands that Middlesex County is conducting a Municipal Class EA for bridge improvements along Thorndale Road, as identified in the information provided. As requested, an initial species at risk (SAR) information screening has been completed under the *Endangered Species Act, 2007* (ESA 2007) by MECP's Species at Risk Branch (SARB) for the above-noted project location with respect to endangered and threated species in Ontario. There are known occurrences of the following endangered or threatened SAR in the general area with potential to occur at the project location: • Silver Shiner (threatened, species and general habitat protection) • Barn Swallow (threatened, species and general habitat protection) Please note that this is an initial screening for endangered and threatened SAR and the absence of an element occurrence does not indicate the absence of species. The province has not been surveyed comprehensively for the presence or absence of SAR and Ontario's data relies on observers to report sightings of SAR. Field assessments by a qualified professional are recommended as there is a high likelihood for SAR species and/or habitat to occur within the project location. Also, attached are some documents that my be helpful to you. The position of SARB is based on the information that has been provided by you on behalf of the County. Should information not have been made available and considered in our review, or new information comes to light, or if on-site conditions and circumstances change, please contact SARB as soon as possible (SAROntario@ontario.ca) to discuss next steps. Regards,	
Public Information Centre 1			
Fisheries and Oceans Canada (DFO) Fisheries Protection Program fisheriesprotection@dfo-mpo.gc.ca	September 12, 2019 Email	Confirmation of receipt received.	No response required
Hon. J. Yurek Member of Provincial Parliament (MPP) Elgin-Middlesex-London 750 Talbot Street, Suite 201	September 12, 2019 Email	Confirmation of receipt received.	No response required

Contact	Date	Comment	Response/ Status
London ON N5P 1E2 Tel: 519-631-0666 Jeff.yurek@pc.ola.org			
Karen Vecchio Member of Parliament (MP) Elgin-Middlesex-London Constituency Office 203-750 Talbot Street St. Thomas, ON N5P 1E2 519-637-2255 Tel: 519-637-2255 Karen.Vecchio@parl.gc.ca	September 12, 2019 Email	Confirmation of receipt received.	No response required
Karina Černiavskaja, District Planner Ministry of Natural Resources and Forestry, Aylmer District 615 John St. N. Aylmer, ON N5H 2S8 Tel: 519-773-4757 Fax: 519-773-9014 karina.cerniavskaja@ontario.ca	September 12, 2019 Email	• The Ministry of Natural Resources and Forestry (MNRF) Aylmer District received the attached Notice of Public Information Centre #1 for the proposed Thorndale Bridge Improvements project on September 12th, 2019. • Email provided general information regarding MNRF legislation, including *Natural Heritage & Endangered Species Act, the Petroleum Wells & Oil, Gas, and Salt Resource Act, Public Lands Act & Lakes and Rivers Improvements Act*	Response sent October 15, 2019 by Stantec (M. Cameron): Thank you for providing comments on the Notice of Public Information Centre 1. Please find attached a letter requesting natural heritage data relevant to the Middlesex County Thorndale Bridge improvements (Municipal Class EA Study). We have completed a preliminary screening based on publicly-available data sources and request your confirmation of our findings or any additional natural heritage data you may have. A copy is also being provided to MECP for their information. Based on our interpretation of MECP's "Draft Proponent's Guide to Preliminary Screening for Species at Risk", consultation with an MECP biologist is initiated once results of field investigations are available and potential project impacts are understood.
Kathleen Buck Management Biologist Ministry of Natural Resources and Forestry, Aylmer District 615 John St. N. Aylmer, ON N5H 2S8 Tel: 519-773-4785 Fax: 519-773-9014 Kathleen.buck@ontario.ca	October 31, 2019 Email	• MNRF email provided in response to the October 15, 2019 email, regarding the preliminary screening of available background data. • MNRF provided additional information regarding the following: • Fish and Fish Habitat: species summary (as provided in the email), thermal regime (Warm), and the in-water timing window (March 15-July 15) • Terrestrial Resources: There are no Areas of Natural and Scientific Interest (ANSIs) or Provincially Significant Wetlands (PSWs) within or adjacent to the project area. • Species of Conservation Concern: The habitat of provincially rare (S1-S3, SH) and Special Concern species is considered Significant Wildlife Habitat under the category of 'Special Concern and Rare Wildlife Species' in the Significant Wildlife Habitat Technical Guide Ecoregion Criteria Schedules. Therefore, consideration should be given to these species and whether their habitat occurs on or within 120 m of the Study Area. • The following Species of Conservation Concern (provincially tracked species) are located in the vicinity of the Study Area. Please note, this does not include species that are listed on the Species at Risk in Ontario (SARO) List. To ensure access to reliable and up to date information, please contact SAROntario@ontario.ca. • Hairy-fruited Sedge (S3), Lizard's-tail (S3), Narrow-leaved Wild Leek (S1?), Striped Cream Violet (S3), Elktoe (S3), Great Egret (S2B), Greater Redhorse (S3), Mucket (S3), Prairie Milkweed (S2S3), Purple Wartyback (S3)	No further response required. Information will be incorporated into the EIS, as required.

Contact	Date	Comment	Response/ Status
		• MNRF noted that it remains the proponents responsibility to complete a preliminary screening for each project, to obtain available information from multiple sources, to conduct any necessary field studies, and to consider any potential environmental impacts that may result from an activity. • MNRF emphasized the need for the proponents of development activities to complete screenings prior to contacting the Ministry or other agencies for more detailed technical information and advice. • MNRF also provided information about data limitations and where to access data to assist with scoping field assessments, as required.	
Public Information Centre 2			
Fisheries and Oceans Canada (DFO) Fisheries Protection Program fisheriesprotection@dfo-mpo.gc.ca	January 28, 2020 Email	Confirmation of PIC 2 Notice received.	No response required
Karen Vecchio Member of Parliament (MP) Elgin-Middlesex-London Constituency Office 203-750 Talbot Street St. Thomas, ON N5P 1E2 519-637-2255 Tel: 519-637-2255 Karen.Vecchio@parl.gc.ca	January 28, 2020 Email	Confirmation of PIC 2 Notice received.	No response required
ilove thorndale info@ilovethorndale.ca	January 30, 2020 Email	Thank you for the PIC 2 notice. We have posted this on our website and will share on our social media	No Response Required
Tilman Jooster Thames Valley Trail Association traildevelopment@tvta.com	February 13, 2020 PIC Comment	All looks good	No Response Required
Alex Vanderkam Thames Valley Trail Association [redacted] president@tvta.com	February 13, 2020 PIC Comment	• Please keep Thames Valley Trail Association updated Glad to see the work being done and consideration for the TVTA trail taken into account	No Response Required

APPENDIX A.6

Indigenous Correspondence

Middlesex County - Thorndale Bridge Improvements – County Road 28 (Thorndale Road)
Municipal Class Environmental Assessment, Schedule "C"
First Nations Consultation Log

Contact	Date/ Method of Communication	Comment/ Concern	Response/ Commitment to Carry Forward
Chippewas of the Thames First Nations Chief Henry Myeengun Ms. Rochelle Smith Updated April 2019 Chief Jacqueline French Fallon Burch Consultation Coordinator 320 Chippewa Road, RR1 Muncey, ON N0L 1Y0 519-289-5555 ext 251 consulation@cottfn.com	Notice of Study Commencement Sent via Canada Post April 8, 2019	Letter was received in response to the NOSC on April 28, 2019. It was sent by Fallon Burch, Chippewa of the Thames, Consultation Coordinator. • Proposal is within Chippewas of Thames First Nation Traditional Territory, as well as the Big Bear Creek Additions to Reserve Land selection area • Have no concerns with project at this time • Requested to be notified if Stage 3 Archaeology Assessment is required	COTTFN to be notified if Stage 3 Archaeology Assessment is required.
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	Spoke with Fallon Burch • She had received our notice of PIC2. She had not gone through it in detail but would follow up with Stantec if she had any questions.	
	Email from Fallon Burch, March 10, 2020	Email with a letter of response to the PIC 2 Notification • Noting the project is located within the McKee Treaty Area to which Chippewas of the Thames First Nation (COTTFN) is a signatory • Project is also within the Big Bear Creek Additions to Reserve (ATR) land selection area as well as COTTFN's Traditional Territory • Minimal concern with the project • Requested to be sent an electronic copy of studies as they are completed to consultation@cottfn.com • If an Archaeological Assessment conducted they require notification and the opportunity to actively participate by sending First Nation Field Liaison on behalf of this First Nation	**Isaac Bartlett sent an email response March 10, 2020** Thank-you for the input Fallon. We will record your responses and will reach out to you regarding the follow-up archaeological study. Send electronic copies of final ESR to consultation@cottfn.com
Oneida Nation of the Thames Chief Jessica Hill Cherilyn Hill Updated April 2019 Brandon Doxatatro - Environmental Coordinator 2210 Elm Avenue, Southwold ON N0L 2G0 2212 Elm Avenue, Southwold ON N0L 2G0 (519) 652-3244 jessica.hill@oneida.on.ca	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	Spoke with Brandon Doxtatro • Notice of PIC 2 was received, had not read it at this time • Please call back on Tuesday	
	Follow up phone call Tuesday February 4, 2020	• Brandon Doxtatro was busy, left name, number and reason for call with secretary	
Munsee-Delaware Nation Chief Roger Thomas Mr. Glenn Forrest Updated April 2019 Stacy Phillips - Consultation Director of Operations/Band Manager 289 Jubilee Road Muncey, On N0L 1Y0	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		

Contact	Date/ Method of Communication	Comment/ Concern	Response/ Commitment to Carry Forward
(519) 289-5396 ext. 222 glenn@munsee.ca	Follow up phone call Friday January 31, 2020	Spoke with Stacy Phillips • He indicated that no response from Munsee-Delaware Nation means no concerns • He had received the notice and read it while on the phone	
Bkejwanong Territory (Walpole Island) Chief Dan Miskokomon Janet Macbeth Project Review Coordinator 117 Tohgohoning Road RR #3 Walpole Island, ON N8A 4K9 (519) 627-1475 (Heritage Centre) Janet.macbeth@wifn.org	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	Spoke with Janet Macbeth • She had received the notice and was thinking of attending the PIC • Interest in participating in the Stage 2 Archaeological Assessment • Interested in SAR mitigation measures and Natural Heritage aspect of the project • During construction they would like to be considered for opportunities for work/ employment • Send ESR for review during 30-day review • Would like to meet with the County of Middlesex and Thames Centre once the design has been finalized in summer of 2020	• Stage 2 Archaeological Assessment – contact Mrs. Macbeth • Send ESR for review during 30 day review • Provide the County of Middlesex and Thames Centre with Mrs. Macbeth's contact information to set up a meeting
Aamjiwnaang First Nation Chief Chris Plain 978 Tashmoo Avenue, Sarnia, ON N7T 7H5 (519) 336-8410 chief.plain@aamjiwnaang.ca	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	• Left a message on Mr. Plain's voice mail	
Caldwell First Nations Ms. Nikki Orosz Director of Operations 14 Orange Street, P.O. Box 388 Leamington ON, N8H 1P5 (519) 322-1766 (ext 2) nikki.orosz@caldwellfirstnation.ca	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	Left a message on Ms. Orosz's voice mail	
Delaware Nation (Moravian of the Thames) Chief Denise Stonefish Consultation Assistant 14760 School House Line RR #13 Thamesville, ON N0P 2K0 (519) 692-3936 denise.stonefish@delawarenation.on.ca	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		

Contact	Date/ Method of Communication	Comment/ Concern	Response/ Commitment to Carry Forward
	Follow up phone call Friday January 31, 2020	• Left a message on Denise Stonefish voice mail	
Chippewas of Kettle and Stony Point First Nations Chief Jason Henry 6247 Indian Lane Lambton Shores N0N 1J2 (519) 786-2125 jason.henry@kettlepoint.org	Notice of Study Commencement Sent via Canada Post April 8, 2019		
	Notice of PIC 2 and PIC 1 panels Sent by email January 29, 2020		
	Follow up phone call Friday January 31, 2020	• Left a message on Mr. Henry's voice mail	

APPENDIX A.7

Notice of Study Completion

(not completed)

Notice of Study Completion
Thorndale Bridge Improvements
Schedule C Municipal Class Environmental Assessment Study

Middlesex County completed a Municipal Class Environmental Assessment (EA) study to plan for improvements to the Thorndale Bridge on County Road 28 (Thorndale Road). The Class EA study identifies the preferred plan as the replacement of the existing bridge with a new bridge. The new bridge will accommodate two 3.75 m travel lanes with 1.6 m paved shoulders on each side, and a 2.5 m raised bike trail on the south side of the bridge. County Road 28 will be closed during construction and a signed detour route for traffic will be used. Construction of the new Thorndale Bridge is recommended in the next 10 years, pending funding, approvals, and coordination with other projects.

The study was completed in accordance with the planning and design process for Schedule C projects, as outlined in the Municipal Class EA document (October 2000, as amended), which is an approved process under the Ontario Environmental Assessment Act. An Environmental Study Report (ESR) summarizing the study process and recommendations is available for public review for 45 calendar days from **July 15, 2021 to August 25, 2021**. In light of COVID-19, the ESR will only be available for review online: https://www.middlesex.ca/departments/environmental-assessments/thorndale-bridge-environmental-assessment

Interested persons may provide written comments to our project team by **August 25, 2021**. All comments and concerns should be emailed directly to Chris Traini, Project Engineer at Middlesex County (ctraini@middlesex.ca) or discussed via telephone (519-434-7321, extension 2347).

In addition, a request may be made to the Ministry of the Environment, Conservation and Parks for an order requiring a higher level of study (i.e., requiring an individual/comprehensive EA approval before being able to proceed), or that conditions be imposed (e.g., require further studies), only on the grounds that the requested order may prevent, mitigate, or remedy adverse impacts on constitutionally protected Aboriginal and treaty rights. Requests on other grounds will not be considered. Requests should include the requester contact information and full name for the ministry.

Requests should specify what kind of order is being requested (request for additional conditions or a request for an individual/comprehensive environmental assessment), how an order may prevent, mitigate, or remedy those potential adverse impacts, and any information in support of the statements in the request. This will ensure that the ministry is able to efficiently begin reviewing the request. The request should be sent in writing or by email to the County of Middlesex and to:

Minister of the Environment, Conservation and Parks
Ministry of Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

and

Director, Environmental Assessment Branch
Ministry of Environment, Conservation and Parks

135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

All personal information included in your request – such as name, address, telephone number and property location – is collected, under the authority of section 30 of the EA Act and is collected and maintained for the purpose of creating a record that is available to the general public. As this information is collected for the purpose of a public record, the protection of personal information provided in the Freedom of Information and Protection of Privacy Act (FIPPA) does not apply (s.37). Personal information you submit will become part of a public record that is available to the general public unless you request that your personal information remain confidential.

This Notice issued July 14, 2021.